



Friday, 6 December 2019

Dear Sir/Madam

A meeting of the Planning Committee will be held on Monday, 16 December 2019 in the Council Chamber, Council Offices , Foster Avenue, Beeston NG9 1AB, commencing at 7.00 pm.

Should you require advice on declaring an interest in any item on the agenda, please contact Legal Services at your earliest convenience.

Yours faithfully

Chief Executive

To Councillors: D Bagshaw
L A Ball BEM
T A Cullen
D Grindell
M Handley
R I Jackson
R D MacRae

J W McGrath (Vice-Chair)
P J Owen
D D Pringle
C M Tideswell
D K Watts (Chair)
R D Willimott

AGENDA

1. APOLOGIES

To receive any apologies and notifications of substitutes.

2. DECLARATIONS OF INTEREST

Members are requested to declare the existence and nature of any disclosable pecuniary interest and/or other interest in any item on the agenda.

3. MINUTES

PAGES 1 - 6

To approve the minutes of the meeting held on 6 November 2019.

4. NOTIFICATION OF LOBBYING

5. DEVELOPMENT CONTROL

5.1 19/00610/FUL PAGES 7 - 24

Construct three storey side extension and convert existing house to create 7 apartments, rear dormer, first floor rear extension, demolish existing garage, external alterations, new vehicular and pedestrian access, 7 car parking spaces and bin and cycle stores (revised scheme).

232 Queens Road, Beeston, Nottinghamshire, NG9 2BN

5.2 19/00563/FUL PAGES 25 - 42

Construct two storey apartment block, incorporating 3 two bed units and 1 one bed unit.

Former site of 8th Beeston Scout Hut Headquarters, Waverley Avenue, Beeston, Nottinghamshire, NG9 1HZ

5.3 19/00600/FUL PAGES 43 - 54

Construct three storey apartment building containing 8 apartments, including 8 car parking spaces.

Land on the North West Side of The Man In Space, Nottingham Road, Eastwood, Nottinghamshire, NG16 3GR

5.4 19/00452/FUL PAGES 55 - 74

Change of use from agricultural land to recreational use, conversion of existing barn to coffee and farm shop with site manager's accommodation, decking, parking and provision of an additional 3 glamping pods and retention of bbq hut.

Mill Farm, 62 Mill Road, Stapleford, Nottinghamshire, NG9 8GD

5.5 19/00442/FUL PAGES 75 - 90

Construct two storey side extension and subdivide existing building to form four apartments, construct bin store and erect boundary enclosure and gates.

45 Town Street Bramcote Nottinghamshire NG9 3HH

6. INFORMATION ITEMS

6.1 Appeal Decisions

PAGES 91 - 92

Erect 10 one bedroom apartments.

147 - 151 Queens Road, Beeston, Nottinghamshire, NG9

2FE

6.2 Appeal Statistics

PAGES 93 - 94

Update Report on Appeal Statistics in Relation to the Criteria for Designation of Local Planning Authorities.

6.3 Delegated Decisions

PAGES 95 - 102

7. EXCLUSION OF PUBLIC AND PRESS

The Committee is asked to RESOLVE that, under Section 100A of the Local Government Act, 1972, the public and press be excluded from the meeting for the following item of business on the grounds that it involves the likely disclosure of exempt information as defined in paragraphs 1 and 2 of Schedule 12A of the Act.

8. 19/00693/FUL

PAGES 103 - 114

Two storey and single storey extensions, including side balcony (revised scheme) and retain replacement fence.

The Gables, Strelley Lane, Strelley, Nottinghamshire, NG8

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PLANNING COMMITTEE

WEDNESDAY, 6 NOVEMBER 2019

Present: Councillor D K Watts, Chair

Councillors: D Bagshaw
L A Ball BEM
T A Cullen
D Grindell
M Handley
R D MacRae
J W McGrath
D D Pringle
C M Tideswell
R D Willimott
J C Goold Substitute
P D Simpson Substitute
S J Carr Ex-Officio

Apologies for absence were received from Councillors R I Jackson and P J Owen.

47. DECLARATIONS OF INTEREST

There were no declarations of interest.

48. MINUTES

The minutes of the meeting held on 8 October 2019 were approved as a correct record and signed.

49. NOTIFICATION OF LOBBYING

The Committee received notifications of lobbying in respect of the planning applications subject to consideration at the meeting.

50. DEVELOPMENT CONTROL

50.1. 19/00554/FUL

Construct additional vehicle transporter egress to Nottingham Road (A6005).
Car Store, Nottingham Road, Attenborough, Nottingham, NG9 6DQ

This planning application to construct an additional vehicle transporter egress to Nottingham Road, had been called in by Councillor L Fletcher.

There were no late items for the application.

Mr Philip Morley MBE (supporting) and Councillor L Fletcher (ward member) addressed the Committee prior to the general debate.

Members debated the application and the following comments were amongst those noted:

- The residents of the surrounding area supported the application.
- The proposed egress would alleviate traffic and parking issues to the surrounding area.

RESOLVED that planning permission be granted subject to the following conditions:

1. The development hereby permitted shall be commenced before the expiration of three years beginning with the date of this permission.
2. The development hereby permitted shall be carried out in accordance with the Site Location Plan received by the Local Planning Authority on 28 August 2019 and the amended drawings numbered 19.027/01 and 19.027/02 received by the Local Planning Authority on 18 October 2019.
3. The egress hereby approved shall not be brought into use until the bollards and signs, as shown on the amended drawings numbered 19.027/01 and 19.027/02 received by the Local Planning Authority on 18 October 2019, have been installed and these shall then be retained in this form for the lifetime of the development.
4. The car parking spaces shall remain in accordance with the layout shown on the amended drawing numbered 19.027/01 received by the Local Planning Authority on 18 October 2019. The car parking spaces shall only be used for the purposes indicated on the plan and for no other purpose. No vehicles shall be displayed for sale in the car park outside of the areas shown specifically for that purpose on the approved plan.
5. There shall be no vehicles sited or parked within the grassed/ landscaped areas shown on the amended drawings numbered 19.027/01 and 19.027/02 received by the Local Planning Authority on 18 October 2019.
6. All car transporters accessing the site shall only enter the site from the existing Nottingham road (A6005) access point and shall only exit the site using the Nottingham road (A6005) egress hereby approved. Any unloading or loading of vehicles from a car transporter shall only be carried out within the application site boundary as indicated on the site location plan received by the local planning authority 28 August 2019.

Reasons

1. To comply with S91 of the Town and Country Planning Act 1990 as amended by S51 of the Planning and Compulsory Purchase Act 2004.
2. For the avoidance of doubt.

3. In the interests of highway safety, to prevent customers and staff using the egress and in accordance with the aims of Policy 17 of the Part 2 Local Plan (2019).
4. To ensure adequate parking and access is available within the site and to ensure an acceptable standard of appearance in accordance with Policy 10 of the Broxtowe Aligned Core Strategy (2014) and Policy 17 of the Part 2 Local Plan (2019).
5. To ensure the development presents a pleasant appearance in the locality and in accordance with Policy 10 of the Broxtowe Aligned Core Strategy (2014) and Policy 17 of the Part 2 Local Plan (2019).
6. In the interests of highway safety and neighbour amenity and in accordance with the aims of Policy 10 of the Broxtowe Aligned Core Strategy (2014) and Policy 17 of the Part 2 Local Plan (2019).

Notes to applicant

1. The council has acted positively and proactively in the determination of this application by working to determine it within the agreed determination timescale.
2. The development makes it necessary to construct a vehicular crossing within the public highway. These works shall be constructed to the satisfaction of the highway authority. Works will be subject to a design check and site inspection for which a fee will apply. The application process can be found at:
<http://www.nottinghamshire.gov.uk/transport/licences-permits/temporary-activities>
3. There is operational gas apparatus within the application site boundary. You are required to contact Cadent's Plant Protection Team for approval before carrying out any works on site. Email: plantprotection@cadentgas.com Tel: 0800 688 588

50.2. 19/00295/FUL

Change of use of 6 bed house in multiple occupation (Class C4) to a 7 bed house in multiple occupation.

56 Salisbury Street, Beeston, Nottingham, NG9 2EQ

This planning application for the change of use of 6 a bed house in multiple occupation (Class C4) to a 7 bed house in multiple occupation, had been called in by Councillor P Lally.

There were no late items for this application.

Mrs Suzanne Watt (objecting) and Councillor L A Lally (ward member) addressed the Committee prior to the general debate.

Members debated the application and the following comments were amongst those noted:

- The proposal was over intensive for the surrounding area which would be out of keeping with the residential character of the area.
- Further parking issues that would arise from the increased number of residents.
- There was a decline in housing mixture.

RESOLVED that the application be refused.

Reason

The proposed development by virtue of the intensity of occupation would be out of keeping with the residential character of the area, and would result in unacceptable parking problems in the area due to the insufficient provision of on-site parking, contrary to Policy 10 of the Broxtowe Aligned Core Strategy 2014 and Policy 17 of the Part 2 Local Plan 2019.

50.3. 19/00477/REG3

Construct 3 Industrial Units (Class B2)
Mushroom Farm Court, Meadowbank Way, Eastwood, Nottingham, NG16 3SR

The application, which sought permission to Construct 3 Industrial Units (Class B2), was considered by the Committee.

Members were asked to consider the late items for the application, which included an occupier of the existing units raising concern about the access for vehicles, removal of the walkway and the loss of parking.

Members debated the application and the following comments were amongst those noted:

- Unit 8 of the proposed application was too wide.
- There was a loss of parking.
- The development would be beneficial for the regeneration of Eastwood.

RESOLVED that planning permission be granted subject to the following conditions.

- 1. The development hereby permitted shall be commenced before the expiration of three years beginning with the date of this permission.**
- 2. The development hereby permitted shall be carried out in accordance with the drawings numbered CW20:008:001 (1:1250), CW20:008:003 Rev.A (1:500), CW20.008.004 Rev.A (1:100), CW20.008.006 Rev.A (1:100), CW20.008.005 Rev.A (1:100); received by the local planning authority on 26 July and 5 September 2019.**
- 3. The development hereby approved shall be constructed using materials to match the existing units on the application site.**

4. No part of the development hereby approved shall be commenced until details of appropriate gas prevention measures have been submitted to and approved in writing by the local planning authority. No building to be completed pursuant to this permission shall be occupied or brought into use until:
 - i) all appropriate measures have been completed in accordance with details approved in writing by the local planning authority; and
 - ii) it has been certified to the satisfaction of the local planning authority that necessary remedial measures have been implemented in full.
5. The 3 additional Industrial units hereby permitted shall not be brought into use until the parking, turning and servicing areas are surfaced in a bound material with the parking bays clearly delineated in accordance with drawing number CW20:008:003 REV A. The parking, turning and servicing areas shall be maintained in the bound material for the life of the development and shall not be used for any purpose other than the parking, turning and loading and unloading of vehicles.

Reasons

1. To comply with S91 of the Town and Country Planning Act 1990 as amended by S51 of the Planning and Compulsory Purchase Act 2004.
2. For the avoidance of doubt.
3. To ensure a satisfactory standard of appearance is achieved in accordance with Broxtowe Aligned Core Strategy (2014) Policy 10.
4. In the interest of public health and safety.
5. To ensure that adequate off-street parking provision is made to reduce the possibilities of the proposed development leading to on-street parking in the area.

Notes to applicant

1. The council has acted positively and proactively in the determination of this application by working to determine it within the agreed determination timescale.
2. The proposed development lies within a coal mining area which may contain unrecorded coal mining related hazards. If any coal mining feature is encountered during development, this should be reported immediately to the Coal Authority on 0345 762 6848.
3. Further information is also available on the Coal Authority website at: www.gov.uk/government/organisations/the-coal-authority

51. PLANNING ENFORCEMENT PLAN

The Committee considered the proposed amendments to the Planning Enforcement Plan. The Plan had been updated to reflect the suggestions made by the Planning Committee 24 July 2019. These were:

- Paragraph 6.3 - Officers will always endeavour to gain sufficient evidence to support a case without reliance upon the complainant, however there may still be circumstances in which evidence from complainants is also required, for example for noise complaints where a noise diary can be used as evidence.
- Paragraph 6.4 - Anonymous complaints will be considered on a case by case basis and officers will take steps to encourage complainants who do not wish to provide their details by providing assurances regarding confidentiality.
- Paragraph 8.4 - Enforcement complaints will be registered and allocated to an officer within 5 working days.
- Paragraph 13.1 - Any action to be undertaken will be in liaison with the Chair and Vice Chair of Planning Committee and Ward Councillors, in accordance with the Council's Scheme of Delegation.

RESOLVED that the Planning Enforcement Plan be approved.

52. INFORMATION ITEMS

52.1. APPEAL STATISTICS - UPDATE REPORT ON APPEAL STATISTICS IN RELATION TO THE CRITERIA FOR DESIGNATION OF LOCAL PLANNING AUTHORITIES.

The Committee noted that the position remained unchanged from that reported at its meeting on 4 September 2019. The Council was not therefore currently at risk of special measures based on the figures reported to the Committee on the aforementioned date.

52.2. APPEAL DECISIONS

The Appeal Decisions were noted.

52.3. DELEGATED DECISIONS

The delegated decisions were noted.

Report of the Chief Executive

APPLICATION NUMBER:	19/00610/FUL
LOCATION:	232 QUEENS ROAD, BEESTON, NOTTINGHAMSHIRE, NG9 2BN
PROPOSAL:	CONSTRUCT THREE STOREY SIDE EXTENSION AND CONVERT EXISTING HOUSE TO CREATE 7 APARTMENTS, REAR DORMER, FIRST FLOOR REAR EXTENSION, DEMOLISH EXISTING GARAGE, EXTERNAL ALTERATIONS, NEW VEHICULAR AND PEDESTRIAN ACCESS, 7 CAR PARKING SPACES AND BIN AND CYCLE STORES (REVISED SCHEME)

Councillor P Lally has requested this application be determined by Planning Committee.

1 Executive Summary

- 1.1 The application seeks planning permission to construct a three storey side extension and to convert the existing house to create seven, self-contained apartments which are not HMO's (House in Multiple Occupancy). A three storey, partially glazed link with a pitched and flat roof will connect the main property and extension. The existing garage will be demolished and the existing single storey rear extension will be increased in width by 2m. A flat roof dormer will be constructed in the rear roof slope of the main house. A vehicular and pedestrian access will be created and provision for seven car parking spaces and a cycle store will be included.
- 1.2 This is a revised application following the refusal of an application (19/00272/FUL) for nine apartments at planning committee in July 2019.
- 1.3 The main property is a traditional style, three storey, semi-detached property on a corner plot. It has ground and first floor bay windows and an attached flat roof single storey rear extension and garage projecting to the side. The roof slopes down to the rear from three storeys to two storeys and has a two storey rear projecting element with mono-pitched roof.
- 1.4 The main issues relate to whether the principle of converting and extending the property to create seven apartments would be acceptable, if the development is acceptable in terms of flood risk, parking issues and whether there will be an unacceptable impact on neighbour amenity.
- 1.5 The benefits of the proposal would mean six additional homes within a sustainable, urban location with access to regular sustainable transport links which would be in accordance with policies contained within the development plan which is given significant weight. The proposed works would contribute to the local economy by providing jobs during the construction process. There would be some impact on neighbour amenity and available parking but these matters are considered to be outweighed by the benefits of the scheme.

1 Details of the Application

- 1.1 The application seeks planning permission to construct a three storey side extension and to convert the existing house to create seven, self-contained apartments which are not HMO's (House in Multiple Occupancy). A three storey, partially glazed link with a pitched and flat roof will connect the main property and extension. The existing garage will be demolished and the existing single storey rear extension will be increased in width by 2m. A flat roof dormer will be constructed in the rear roof slope of the main house. A vehicular and pedestrian access will be created and provision for seven car parking spaces and a cycle store will be included.
- 1.2 The main differences between this application and the previously refused application (19/00272/FUL) consist of a reduction in the number of apartments by two, increase of one car parking space, removal of the existing garage (previously proposed a first floor extension above with dormer), replacement of gable features with double height bay windows in the south east (front) elevation of the extension, amendments to the fenestration, reduction in footprint of the extension so it aligns with the front elevation of the main house and the rear elevation has been reduced in projection by approximately 0.7m. The proposed block plan shows a purple dashed line outlining the previous scheme in relation to the reduced sized scheme.
- 1.3 The proposed seven apartments will consist of four, two bedroom apartments and three, one bedroom apartments. In addition to the bedrooms, each apartment will have a kitchen/living/dining area and one or two bathrooms/en-suites. Seven parking spaces are proposed to the rear of the three storey extension (accessed from Dagmar Grove) and the cycle store will provide space for approximately three bikes underneath the cantilevered roof.

2 Site and surroundings

- 2.1 The site contains a three storey, semi-detached house with rear extensions positioned on a corner plot. The house is constructed from red brick and grey clay tiles. A two storey extension with mono-pitched roof extends to the rear and adjoins a small single storey flat roof extension and a flat roof garage. There is parking for two cars on site (one space in garage) which is accessed from Dagmar Grove. The site is enclosed by a 2m high fence to the south west which extends across part of the north west boundary. The north west boundary is enclosed by the garage on site and adjoining garage belonging to no. 230. No. 230's garden extends in an L-shape to the north west of the site and two outbuildings in this garden are positioned along the boundary with the site. A 2m high rendered wall and curved top fence extends across the south east boundary of the site (beside Queens Road).
- 2.2 The main house adjoins no. 230 to the north east which is relatively similar in style and scale. No. 6 Dagmar Grove is positioned to the north west of the site and is a semi-detached house with a first floor blank south east (side) elevation with a garden that projects to the north east. No. 234 is a three storey end terrace property positioned on a corner plot to the south west. Nos. 231 and 233 are detached properties positioned to the south east.

- 2.3 The site lies within a predominantly residential area with some commercial units. The site is within walking distance of Beeston town centre with a regular tram

service. A bus stop is positioned directly to the north east of the site on Queens Road, served by a regular bus service. The site is within a reasonable proximity to Beeston train station. The site is relatively flat and is located within Flood Zone 3 which is land with a high probability (between 1 in 100 or greater) of river flooding.

3 Relevant Planning History

- 3.1 An application for a garage and shower room (77/00444/FUL) was granted permission in July 1977.

- 3.2 An application for a three storey side extension and to convert the existing house to create nine apartments with a first floor garage extension, dormers, external alterations, new vehicular and pedestrian access, 6 car parking spaces and cycle store (19/00272/FUL) was refused permission in July 2019 for the following reason: *“The proposed development would be over-intensive with insufficient parking and the extension would not appear subservient to the existing building. A significant loss of neighbour amenity would result from the proposal. Accordingly, the proposal would be contrary to Policies H4, H7 and T11 of the Broxtowe Local Plan (2004), Policy 10 of the Aligned Core Strategy (2014) and Policy 17 of the Draft Part 2 Local Plan (2018).”* This refusal is subject to an appeal.

4 Relevant Policies and Guidance

4.1 **Greater Nottingham Aligned Core Strategies Part 1 Local Plan 2014:**

- 4.1.1 The Council adopted the Core Strategy (CS) on 17 September 2014.

- Policy A: Presumption in Favour of Sustainable Development
- Policy 1: Climate Change
- Policy 2: The Spatial Strategy
- Policy 8: Housing Size, Mix and Choice
- Policy 10: Design and Enhancing Local Identity
- Policy 14: Managing Travel Demand
- Policy 20: Air Quality

4.2 **Part 2 Local Plan**

- 4.2.1 The Council adopted the Part 2 Local Plan on 16 October 2019.

- Policy 1: Flood Risk
- Policy 15: Housing Size, Mix and Choice
- Policy 17: Place-making, Design and Amenity

4.3 **National Planning Policy Framework (NPPF) 2019:**

- Section 2 – Achieving Sustainable Development
- Section 4 – Decision-making
- Section 5 – Delivering a Sufficient Supply of Homes
- Section 11 – Making Effective Use of Land

- Section 12 – Achieving Well-designed Places
- Section 14 – Meeting the Challenge of Climate Change, Flooding and Coastal Change

5 Consultations

- 5.1 **Council's Environmental Health Officer:** raises no objection subject to a condition requiring implementation of noise mitigation measures (as stated within the noise report) prior to development starting. An advisory in respect of working hours has been advised.
- 5.2 **Council's Waste and Recycling Officer:** raises no objection.
- 5.3 **Environment Agency:** raise no objection. They have advised that the floor levels should be set no lower than 27.81m AOD (Above Ordnance Datum), flood resilient and mitigation measures as stated in section 6.1.2 of the FRA are carried out, future occupants should register to receive flood warnings and the submitted Flood Warning and Evacuation Plan should be brought into place prior to occupation.
- 5.4 **Nottinghamshire County Council as Highways Authority:** advise that one parking space per apartment is sufficient, particularly given the site's close proximity to regular transport services and local amenities. The junction onto the A6005 is protected with double yellow lines, and there have there been no recorded collisions along Dagmar Grove for over 10 years. They have advised that the recommended conditions on the previous application (19/00272/FUL) are still relevant.
- 5.5 34 neighbouring properties were consulted and a site notice was displayed. 14 objections and one observation were received and can be summarised as follows:
- Loss of light
 - Loss of privacy
 - Could accommodate up to 22 residents (increasing noise and disturbance) and additionally visitors
 - Not subservient to main house and visually overbearing
 - Out of proportion and dominates corner
 - Overdevelopment of plot
 - Extension is taller than neighbouring property
 - Not in keeping with design/appearance of surrounding houses
 - Out of character with existing residential area and traditional Victorian house it is attached to
 - Building is still too large following amendments
 - Considerably taller than surrounding buildings
 - Increase in traffic
 - Highway safety concerns with cars entering and exiting site
 - Lack of parking
 - Increase in parking and congestion on Dagmar Grove
 - Site visits of the street should be undertaken at different times of the day and week to assess the parking
 - Footpaths will be obstructed
 - Dagmar Grove is used as an unofficial park and ride by those using trains, trams and buses

- Loss of family home
- Not geographically suited to the area
- Error on plan not showing seventh apartment
- The meaning of 'duplex' is unclear
- Difficult to understand how the garden/garage can accommodate an extension, cycle/bin store and 7 parking spaces.
- Contradicts policy in relation to design, height, massing, character of the area and domination of student households
- Unclear why such a large development is needed for this site as the Council has set out land allocations for housing
- Large volume of development in the area including the Queens Hotel pub
- Concern over larger properties in Beeston being converted into HMO's
- Proposal is financially motivated
- Loss of trees
- Property was surrounded by trees and vegetation which were cut down and the planning statement says the corner plot suffers from heavy traffic noise and overlooking which is an attempt to mislead the Council
- Only one bus service along this part of Queens Road
- High rent prices in Beeston
- Lack of affordable housing for first time buyers
- Concerns over lamppost and bus stop being relocated.

6 Assessment

6.1 The main issues for consideration are whether the proposed development is acceptable in flood risk terms, represents an acceptable design and layout, has sufficient parking, has an acceptable relationship with neighbouring properties and provides an acceptable standard of amenity for future occupants.

6.2 **Principle**

6.2.1 The site is within an existing residential area and provides an opportunity to provide additional housing outside of the Green Belt. There is also a need to boost housing supply which sites such as this can help deliver. The provision of seven apartments is considered to be a benefit in terms of the provision of homes.

6.2.2 Policy 8 encourages a mix of housing tenures, types and sizes. It is considered that the emphasis of the policy is on promoting housing mix rather than preserving the existing character of the street. Queens Road is characterised by varying styles and sizes of properties including houses and flats. This development would add to the housing mix and it is considered that the character of the area would not be harmed to an extent which would justify refusing planning permission.

6.2.3 To conclude, the site is located within an urban location and weight must be given to the need to boost housing supply. It will also provide an additional six residential units within an existing settlement in a highly sustainable location, close to Beeston town centre and public transport links. It is considered the proposed apartments

will not have an adverse effect on neighbour amenity and amendments to the design mean the massing, scale and appearance are considered to be acceptable (as detailed below). The principle of the development is therefore considered to be acceptable.

6.3 Flood Risk

- 6.3.1 The site lies within Flood Zone 3 which is land with a high probability (1 in 100 or greater) of river flooding. A Flood Risk Assessment has been submitted with the application. Paragraphs 155 – 158 of the NPPF state that inappropriate

development in areas of high risk of flooding should be avoided but where it is necessary, should be undertaken without increasing flood risk elsewhere. All plans should apply a sequential, risk-based approach to the location of development in order to steer new development to areas with the lowest risk of flooding. A Sequential Test has also been submitted with the application which concludes that there are no alternative sites available within areas located in zones with a lower probability of flooding.

- 6.3.2 Within Beeston there are substantial areas which are within Flood Zones 2 and 3 but have a high degree of protection against flooding due to the Nottingham Trent Left Bank Flood Alleviation Scheme. Some of these sites may bring forward the opportunity to provide housing in areas of substantial need. Sequentially, it is considered the site is acceptable and it is considered a positive that this location minimises additional development in the Green Belt in Broxtowe. Therefore when assessing whether other sites are 'reasonably available', this site can be viewed as a 'sustainability benefit' and the Green Belt must be treated as a major constraint.

- 6.3.3 The Environment Agency has raised no objection but has suggested conditioning that the finished floor level of the extension should be set no lower than 27.81m AOD and that the flood resilience techniques as stated in the Flood Risk Assessment, such as using flood resilient materials and design techniques to at least 0.3m above the finished floor level, should be incorporated into the development. An advisory will be recommended in regards to the occupants registering to receive flood warning alerts. It is considered that flood risk issues have been sufficiently addressed.

- 6.3.4 To conclude, within Beeston there are substantial areas which are within Flood Zone 3 but have a high degree of protection against flooding due to the Nottingham Trent Left Bank Flood Alleviation Scheme. A failure to permit residential development on sites such as this which are protected by good quality flood defences, and have a site specific FRA demonstrating the development is acceptable on flood risk grounds, will lead to alternative locations being required in less sustainable locations, including the Green Belt. Subject to a suitable condition, it is considered that the development would be compliant with the requirements of the NPPF in relation to flood risk.

6.4 Amenity, Design and Layout

- 6.4.1 During the course of the application, the plans were amended to include provision for glass recycling and to correct an error in the labelling of the number of apartments which has remained as seven.

- 6.4.2 It is acknowledged that no. 230, the adjoining semi-detached house positioned to the north east, will be one of the properties most affected by this proposal. The amendments to the scheme are considered to improve the relationship with no. 230 and therefore, it is considered that the scheme as proposed would not result in a

detrimental impact on this neighbouring property. The most significant amendment in relation to reducing the impact of the development on no. 230 is the omission of two apartments which reduces the intensity of the scheme. Furthermore, the two rear dormers have been replaced with roof lights and the remaining rear dormer in the main roof has been reduced in width by 0.6m to 2.9m. In the north west (rear) elevation, a first floor window has been removed, one repositioned and reduced in size and a ground floor window removed. Although these dormers and windows do not directly overlook no. 230's garden due to the existing intervening extension, it is considered these will improve the relationship with no. 230 and the perception of feeling overlooked. Concerns were raised about the rear dormer in the main house but the applicant is not willing to amend its design and it is considered a reason for refusal could not be substantiated as the dormer does not cause an unacceptable amount of overlooking to the occupants of number 230.

- 6.4.3 To conclude, it is considered the dormer and four first floor roof lights are an acceptable size and positioning and although three of these windows will serve primary rooms, it is considered these are not dissimilar to what is accepted under permitted development and no adverse overlooking will occur as a result. The proposal of first floor windows in the north west (rear) elevation of the extension are considered to be acceptable and will be partially obscured from the view of no. 230's primary amenity space which is positioned to the north east of the existing rear extensions. The previous application (19/00272/FUL) proposed a first floor extension with dormer over the existing garage; this application proposes a removal of the garage entirely meaning an element of openness will be created for part of no. 230's garden. It is acknowledged that no. 230's amenity will be affected by the proposal to a limited degree but the existing two storey element with mono-pitched roof which mirrors that of the application property already provides a level of screening from the proposed extensions and alterations. Furthermore, the extension could be seen as replicating a house with first floor windows and roof lights in the rear elevation so this type of relationship is not uncommon in an urban area such as this. It is considered there will not be an unacceptable detrimental impact on the amenity of the occupants of no. 230 and the amendments to this scheme in relation to the previous scheme (19/00272/FUL) are acceptable.
- 6.4.3 No. 6 Dagmar Grove is a semi-detached house positioned to the north west of the application site which is the other property that will also be most affected by the proposal. No. 6 Dagmar Grove does not directly adjoin the site and is separated by no. 230's garage and garden. It has a first floor blank south east (side) elevation and a door and two windows at ground floor level. However, these windows and door are obscured from view of the application site by the garage belonging to no. 230. No. 6 Dagmar Grove's garden projects to the north east which adjoins no. 230's rear garden and not the application site. The proposed north west (rear) elevation of the extension will be a minimum of 17m from the south east (side) elevation of no. 6 Dagmar Grove. The three storey extension will largely face the south east (side) elevation of no. 6 Dagmar Grove. Taking the above into consideration, the separation distance and intervening garden, garage and driveway of no. 230, it is considered the proposed extension and alterations will not have a detrimental impact on no. 6 Dagmar Grove.
- 6.4.4 Whilst it is accepted the building will be closer to the boundary with Dagmar Grove and will have side facing windows at ground, first and second floor level, the north east (side) elevation of no. 234 has one first floor window which is obscurely glazed and due to the intervening road, it is considered this relationship is acceptable and

reflects similar relationships of residential properties in the area being built close to a corner boundary. Therefore, it is considered there will not be a detrimental impact on the amenity of the occupants of no. 234.

- 6.4.5 It is considered the proposed extension and alterations are a sufficient distance from other neighbouring properties that there will not be a detrimental impact on their amenity.
- 6.4.6 The overall style of the property reflects a contemporary appearance which is considered to be an acceptable design approach. Although the extension is considered to be large, the design is not dissimilar to a pair of semi-detached dwellings on this spacious corner plot and therefore, it is considered to some extent it retains the appearance of a residential family home. Furthermore, a contemporary design is a widely accepted design approach in Broxtowe Borough and the surrounding properties are considered not to be of an architectural merit that this building would detract therefrom.
- 6.4.7 The layout of the three storey extension aligns with the front bay window and main rear elevation of the host dwelling meaning the extension will not be visually overbearing. Whilst it is acknowledged the three storey extension is significantly wider than the host dwelling, the extension could be read as a pair of semi-detached properties as the host dwelling and no. 230 are, and that a degree of symmetry is reflected between these properties. The courtyard to the rear and cantilevered roof will retain an element of openness to the scheme when viewed from Dagmar Grove which is considered to be a positive element of the scheme.
- 6.4.8 The materials have not been stated on the plans and therefore a condition will be included to ensure that details and samples are provided in advance of building works commencing.
- 6.4.9 It is considered the application has been reduced in footprint, scale and number of apartments that the amenity of surrounding neighbours will not be significantly compromised.
- 6.4.10 The apartments are considered to be an acceptable size with each providing sufficient internal space to provide living/kitchen/dining rooms and bedrooms. Furthermore, each apartment has appropriately positioned windows to provide an outlook from each bedroom.
- 6.4.11 The site is not in an Air Quality Management Area. In addition, the approval of residential developments that are within walking distance of tram, train and bus provision is considered to have a positive benefit on wider air quality issues by encouraging public transport use and therefore reducing the need to travel by private car.
- 6.4.12 Policy 10 of the Aligned Core Strategy states that development should be assessed in relation to its massing and scale, materials, design and impact on the amenity of nearby residents. It is considered the design of the property, whilst contemporary, relates to the massing of the host dwelling and no. 230 with a matching eaves and ridge height and reflecting a degree of symmetry. The development has been reduced in scale and is considered to not be dissimilar to the size of a pair of semi-detached dwellings. It is considered the design is acceptable overall.

6.4.13 To conclude, the proposed extension is positioned on a relatively spacious corner plot and the proposal to extend the existing property and subdivide this into seven apartments is considered to be acceptable. Furthermore, it is considered the scheme has incorporated sufficient amendments to reduce the scale and intensity for this plot of land since the previous application (19/00272/FUL) that an acceptable relationship with neighbours has been achieved. Although the style would be contemporary, it is considered this is an acceptable design approach and the architectural style of the surrounding dwellings is not of such merit that there

would be a requirement to replicate or that a deviation from their design would necessarily be harmful to the character of the area.

6.5 **Parking**

6.5.1 It is evident within the consultation responses that there is concern that the development does not include sufficient parking provision and that this will lead to increased demand for on-street parking which would be detrimental to the area. However, it is considered the amount of parking proposed (seven spaces, one space for each apartment) is acceptable and due to a lack of objection from the Highways Authority and the sustainable location, it is considered the development is acceptable in regards to parking and highway safety.

6.5.2 In relation to assessing the highway impacts of a proposal, paragraph 109 of the National Planning Policy Framework states that development should only be refused on transport grounds where the residual cumulative impacts are severe. Whilst paragraph 105 refers to the setting of local parking standards rather than the determination of planning applications, it provides a list of factors which should be taken into account, including the availability of and opportunities for public transport and the type, mix and use of the development. Policy 10 of the Aligned Core Strategy states that development should be designed to reduce the dominance of motor vehicles.

6.5.3 The site lies within a sustainable location with access to regular bus services along Queens Road (with a bus stop being positioned directly in front of the site) and within walking distance of Beeston town centre which provides access to a regular tram service. It is acknowledged that the properties on Dagmar Grove do not have off-street parking and the main concern would be that there would be an increase in on-street parking along this road. However, seven car parking spaces have been provided and the Highways Authority have not raised any objection in relation to the development, especially considering its sustainable location. It is considered likely that car ownership associated with the apartments will be low. However, it is acknowledged that there could be an increase in parking in the surrounding area but for the reasons set out above, it is considered this would not be detrimental to

parking, traffic or highway safety. Therefore, it is considered that a pragmatic approach needs to be taken in respect of developing the site for residential development.

6.6 **Other issues**

6.6.1 A number of concerns have been raised within the consultation process which will be addressed within this section.

- 6.6.2 Trees have been removed from site but as they were not protected by Tree Preservation Orders and the site is not located within a conservation area, consent would not have been required.
- 6.6.3 Two of the apartments are labelled as 'duplex' apartments as they are situated over ground and first floor, connected by a staircase.
- 6.6.4 Any potential obstruction of the footpaths by works on-site should be reported to Nottinghamshire County Council as Highway Authority.
- 6.6.5 The error on the plans in regards to the numbering of the flats has been corrected.
- 6.6.6 Property/rent prices and if the application is financially motivated are not planning matters that can be taken into consideration with this application
- 6.6.7 The repositioning of the lamppost would be a matter dealt with by Nottinghamshire County Council as Highway Authority. There is no proposal to relocate the bus stop as a result of this application.
- 6.6.8 Although the Council has allocated land for housing within the Part 2 Local Plan, it is considered the proposal is acceptable on this site for the reasons stated in the report. Furthermore, this site is located outside the Green Belt in a highly sustainable area which is considered a benefit of the scheme.
- 6.6.9 The apartments are not specifically targeted at one demographic. The Design and Access Statement states that the different mixture of one and two bedroom apartments could accommodate families, professionals and students. It is therefore considered this development is compliant with Policy 8 of the Aligned Core Strategy which highlights that residential development should maintain, provide and contribute to a mix of housing tenures, types and sizes. Furthermore, the justification of this policy recognises that the projection of smaller households is likely to continue to grow. Therefore, it is considered, developments such as this can cater for this growing need for smaller dwellings.
- 6.6.10 The proposal is for self-contained apartments and not for HMO accommodation.

7 Planning Balance

- 7.1 The benefits of the proposal are that it would provide six additional homes within an existing urban area and would support short term benefits such as jobs during the construction of the proposed dwellings and would be in accordance with policies contained within the development plan. Whilst it is acknowledged there will be
- 7.2 some impact on the amenity of neighbours and on-street parking, this is outweighed by the benefits of the scheme and due to its location within a highly sustainable area.

8 Conclusion

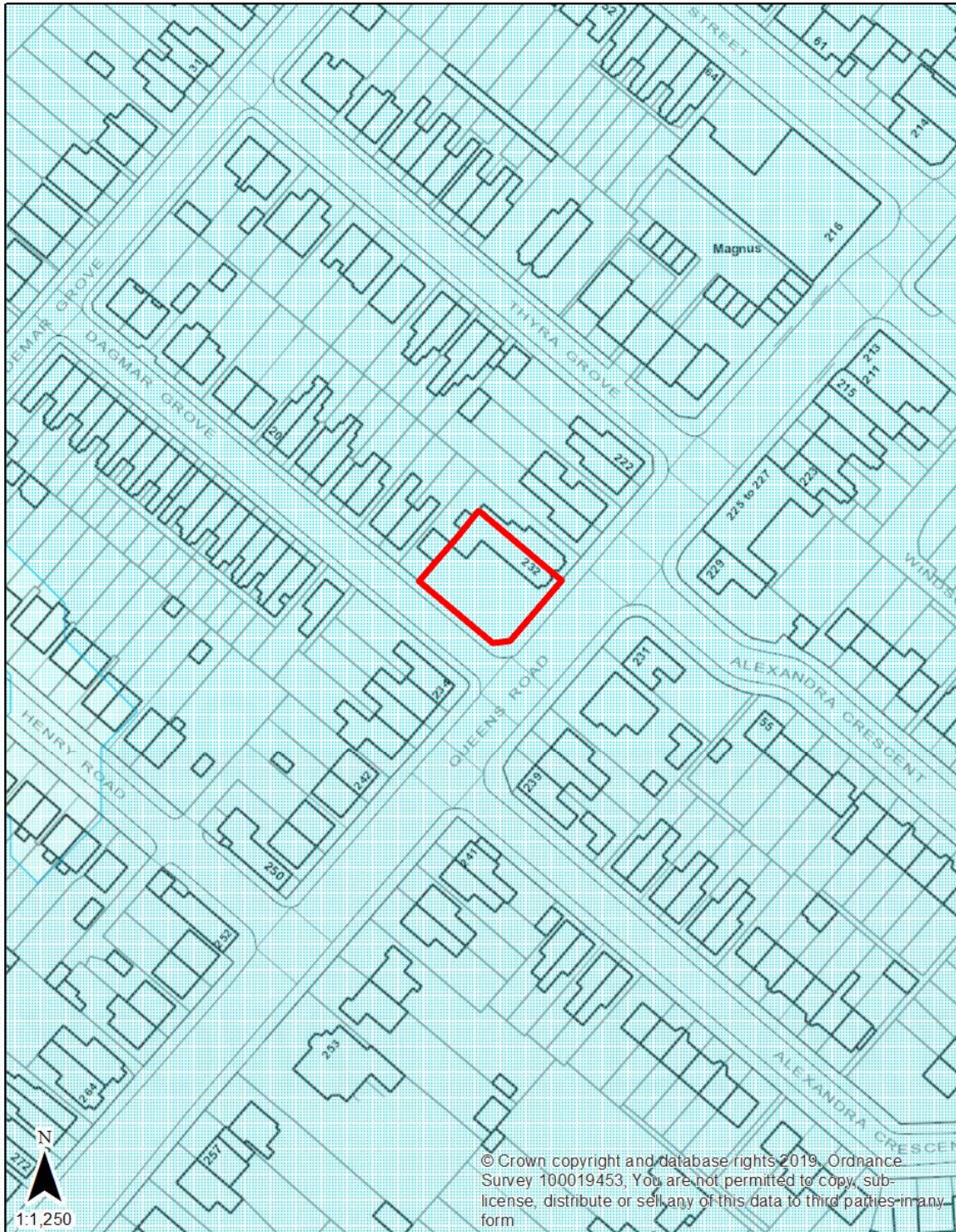
- 8.1 To conclude, it is considered the proposed extensions and alterations are of an acceptable size, scale and design that there will not be an adverse effect on neighbour amenity and provide an acceptable standard of amenity for future occupants of the apartments. The proposed apartments are considered to be an

acceptable design as the height of the extension relates to the host property and the contemporary design is considered to be acceptable.

<u>Recommendation</u>	
<p>The Committee is asked to RESOLVE that planning permission be granted subject to the following conditions.</p>	
1.	<p>The development hereby permitted shall be commenced before the expiration of three years beginning with the date of this permission.</p> <p><i>Reason: To comply with S91 of the Town and Country Planning Act 1990 as amended by S51 of the Planning and Compulsory Purchase Act 2004.</i></p>
2.	<p>The development hereby permitted shall be carried out in accordance with the Site Location Plan (1:1250) and drawing numbers: 232-19-2002 Rev P06 received by the Local Planning Authority on 25 September 2019, 232-19-2001 Rev P05 received by the Local Planning Authority on 29 October 2019 and 232-19-3001 Rev P11 received by the Local Planning Authority on 11 November 2019.</p> <p><i>Reason: For the avoidance of doubt.</i></p>
3.	<p>No part of the development hereby approved shall be commenced until a noise report, prepared in accordance with the provisions of DoT Calculation of Road Traffic Noise to predict noise levels at the nearest noise sensitive location has been submitted to and approved in writing by the Local Planning Authority. Predictions shall be contained in a report which sets out a large scale plan of The proposed development, noise sources and measurement/prediction points marked on plan, a list of noise sources, a list of assumed noise emission levels, details of noise mitigation measures, a description of noise calculation procedures, noise levels at a representative sample of noise sensitive locations and a comparison of noise level with appropriate current criteria. Mitigation measures should be implemented where criteria is exceeded at any location.</p> <p>The dwellings shall not be occupied until all necessary mitigation measures have been implemented.</p> <p><i>Reason: To protect future occupiers from excessive environmental noise and in accordance with Policy 17 of the Part 2 Local Plan (2019).</i></p>
4.	<p>No above ground works, including site clearance, shall take place until a landscaping scheme has been submitted to and approved in writing by the Local Planning Authority. This scheme shall include the following details:</p>

	<ul style="list-style-type: none"> • numbers, types, sizes and positions of proposed trees and shrubs • details of boundary treatments; • proposed hard surfacing treatment and • planting, seeding/turfing of other soft landscape areas. <p>The approved scheme shall be carried out strictly in accordance with the approved details and shall be carried out not later than the first planting season following the substantial completion of the development and any trees or plants which, within a period of 5 years, die, are removed or have become seriously damaged or diseased, shall be replaced in the next planting season with ones of similar size and species to the satisfaction of the Local Planning Authority, unless written consent has been obtained from the Local Planning Authority for a variation.</p> <p><i>Reason: No such details were submitted with the application and to ensure the development presents a satisfactory standard of external appearance to the area and in accordance with the aims of Policies 10 and 11 of the Broxtowe Aligned Core Strategy (2014) and Policy 17 of the Part 2 Local Plan (2019).</i></p>
5.	<p>No above ground works shall commence until samples of external facing materials have been submitted to and approved in writing by the Local Planning Authority. The development shall be constructed only in accordance with the approved details.</p> <p><i>Reason: No such details were submitted with the application and in the interests of the appearance of the development and in accordance with the aims of Policy 10 of the Aligned Core Strategy (2014) and Policy 17 of the Part 2 Local Plan (2019).</i></p>
6.	<p>Prior to the first occupation of the apartments hereby approved, the development shall be constructed in accordance with the submitted Flood Risk Assessment prepared by Inspire Design & Development ref: SS-1423-01-FRA-001 dated April 2019. Flood resilient materials and design techniques shall be used as detailed in paragraph 6.1.2 and the finished floor levels shall be set no lower than 27.81m AOD. These mitigation measures shall be maintained and retained for the lifetime of the development.</p> <p><i>Reason: To reduce the risk of flooding and in accordance with the aims of Policy 1 of the Broxtowe Aligned Core Strategy (2014).</i></p>
7.	<p>The development hereby approved shall not be brought into use until:</p> <ul style="list-style-type: none"> • dropped vehicular footway crossing has been provided and existing dropped vehicular footway has been made redundant in accordance with the Highway Authority's specification. • the parking area has been surfaced in a hard, bound material and each space has been clearly delineated as shown on drawing 232-19-2001 Rev P05 and

	<ul style="list-style-type: none"> the cycle stands have been provided and are available for use. <p>These measures shall be maintained for the lifetime of the development.</p> <p><i>Reason: In the interests of highway safety and in accordance with the aims of Policy 17 of the Part 2 Local Plan (2019).</i></p>
	NOTES TO APPLICANT
1.	The Council has acted positively and proactively in the determination of this application by working to determine it within the agreed determination timescale.
2.	The prospective building manager/occupants should register to receive flood warnings. The Flood Warning and Evacuation Plan for the site should be brought into place prior to occupation of the development, with this information being made available to occupants.
3.	The applicant is advised to contact the Council's Waste and Recycling Section (0115 917 7777) to discuss waste and refuse collection requirements.
4.	Contractors should limit construction and demolition works to between 07:30 and 18:00 Monday to Friday, 08:00 and 13:30 on Saturdays and at no times on Sundays and bank holidays. No waste from the development shall be burnt on site at any time.
5.	The development makes it necessary to construct a vehicular crossing over a footway of the public highway. These works shall be constructed to the satisfaction of the Highway Authority. You are, therefore, required to contact the County Council's Customer Services for on telephone 0300 500 80 80 to arrange for these works to be carried out.



Legend

-  Site
-  Flood Zone 3
-  Flood Zone 2

South west (side) elevation of main house



South west (side) elevation of main house



View of no. 230 and application site from rear garden of no. 6 Dagmar Grove



South east (front) elevation of site and main house and no. 230



View of no. 234 Queens Road and entrance to Dagmar Grove

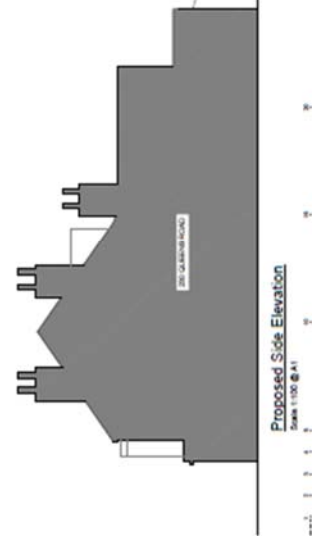
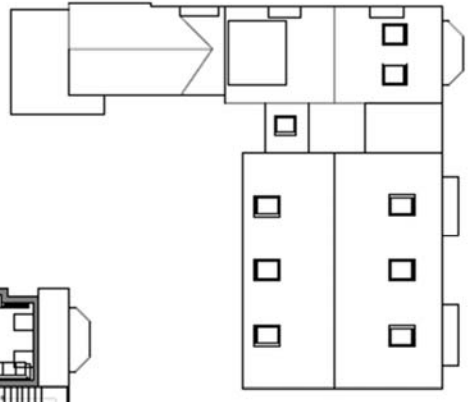
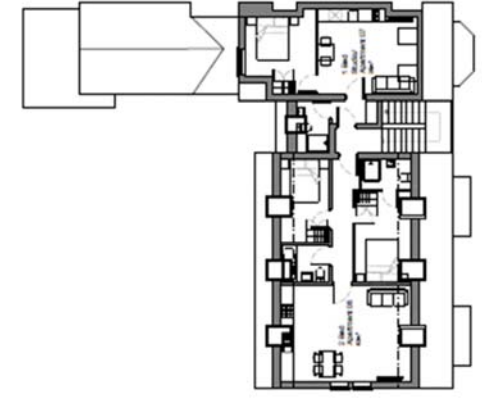
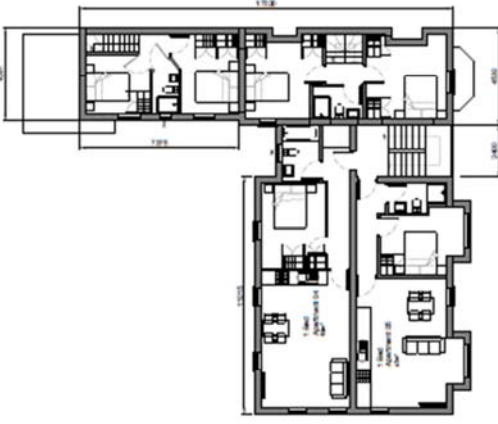
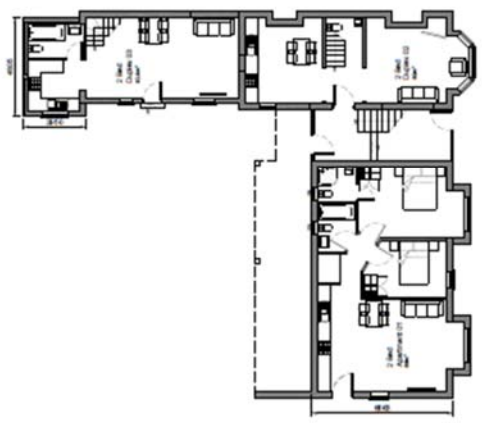


South east (front) elevation of main house and side garden



NOTES

1. THIS IS AN AT DRAWING, IF DIMENSIONS ARE INDICATED IN ANY OTHER FORMAT, THE SCALE SHOWN SHALL BE USED TO INTERPRET THE DRAWING.
2. ALL DIMENSIONS ARE TO FACE UNLESS OTHERWISE SPECIFIED.
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1:10	1:100	1:200	1:500	1:1000	1:2000	1:5000	1:10000
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1:10	1:100	1:200	1:500	1:1000	1:2000	1:5000	1:10000
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1:10	1:100	1:200	1:500	1:1000	1:2000	1:5000	1:10000
1:10	1:100	1:200	1:500	1:1000	1:2000	1:5000	1:10000
1:10	1:100	1:200	1:500	1:1000	1:2000	1:5000	1:10000

Project: 332 Queens Road, Breston
New Apartments

Drawing Title: Proposed Floor Plans & Elevations

Scale: 1:100 Date: 23.04.19
Drawing Number: 332-119-3001
Revisor: P11

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Report of the Chief Executive

APPLICATION NUMBER:	19/00563/FUL
LOCATION:	FORMER SITE OF 8TH BEESTON SCOUT HUT HEADQUARTERS, WAVERLEY AVENUE, BEESTON
PROPOSAL:	CONSTRUCT TWO STOREY APARTMENT BLOCK, INCORPORATING 3 TWO BED UNITS AND 1 ONE BED UNIT

The application is brought to the Committee at the request of Councillor P Lally.

1 Executive Summary

- 1.1 The application seeks planning permission for the erection of a two storey detached building comprising four apartments with four parking spaces, and cycle and bin storage.
- 1.2 The site was formerly occupied by a Scout Hall, which has since been demolished. The site is now cleared.
- 1.3 The main issues relate to whether the principle of residential development is acceptable, if the design, scale and massing and impact on neighbours is acceptable, and the impact on highway safety and parking.
- 1.4 The benefits of the proposal are that it would see the redevelopment of a vacant site, would provide housing in an existing urban area and would be in accordance with the policies contained within the development plan. This is given significant weight.
- 1.5 The negative impact is the potential for an increase in on-street parking in the surrounding area.
- 1.6 The Committee is asked to resolve that planning permission be granted subject to the conditions outlined in the appendix.

APPENDIX

1 Details of the Application

- 1.1 The proposal seeks to build a two storey detached building containing four apartments (three x two bedrooms, and one x one bedroom). The building is proposed to be positioned to front the Queens Road / Station Road junction, and would have a centrally located glazed entrance, via steps, from the corner. The building would have two pitched roofs, with a flat roof between, set back from the first floor roof edge. During the course of the application, the building footprint has been reduced, and as a result, the distance between the rear elevation and the side elevation of 4 Waverley Avenue has been increased by 1m (to 9.1m).
- 1.2 Vehicle access to the site would be from Waverley Avenue, utilising the existing access. Parking and servicing would be provided to the rear of the building, with bin and cycle storage now proposed to be adjacent to the north elevation of the building, and accessed from Station Road. Four parking spaces (which has increased by one, following the receipt of amended plans) and four cycle spaces would be provided. One of the parking spaces would be in an enclosed area to the south west of the building, adjacent to the Waverley Avenue boundary. A pedestrian entrance to the rear of the plot would be provided between the proposed building and 198 Station Road.
- 1.3 The building would be faced mainly in red brick, with grey fibre cement cladding panels to either side of the full height glazed entrance and stair core, to the centre of the frontage, and fibre cement slates to the roof and also to part of the side and rear elevation. A brick and railing enclosure is proposed to the frontage of the site.

2 Site and surroundings

- 2.1 The site, irregular in shape, is located at the junction of Station Road (to the north east), Queens Road (to the south east) and Waverley Avenue (to the south west) and was formerly occupied by a scout hall, a detached single storey timber faced building with a pitched roof that sat centrally within the site. Large advertisement hoardings enclosed the site to the Queens Road and Station Road frontage, with timber fencing and gates to the Waverley Avenue boundary. The scout hall has been demolished and the hoardings removed.
- 2.2 There are two dwellings which share a common boundary with the site. 4 Waverley Avenue is to the north west of the site. This is a two storey detached property which has a single storey flat roof garage adjacent to the application site. There are no windows in the side elevation of this property, facing the site.
- 2.3 198 Station Road is to the north of the site. This is a two storey semi-detached property and has a two storey wing at right angles to the main house, at the rear, typical of a Victorian semi. This property has habitable room windows at both ground and first floor within the side elevation of the wing, facing the site.
- 2.4 To the south west of the site, on the opposite side of Waverley Avenue, is 286 Queens Road. This is a two storey end of terrace dwelling and also has a two storey extension to the rear. There are habitable room windows at both floor levels facing

toward the site. Two and three storey dwellings continue south west along this side of the road.

- 2.5 In the wider area there is a mix of uses. To the north east, on the opposite corner of the junction, advertisement hoardings enclose a commercial use behind. There is a retail unit beyond, facing onto Queens Road. Predominately residential uses continue north east along Queens Road. To the east, on the diagonally opposite corner, there are further retail units housed in a two storey building, which turn the corner into Station Road. To the south of the junction, and south east of the site, there is a flat roof single storey building which is occupied by a supermarket. To the south west of the supermarket, further along Queens Road, there are two blocks of three storey apartments.
- 2.6 The site occupies a prominent position within the street scene, and is approximately 800m south of Beeston Town Centre. The site is within Flood Zones 2 and 3.

3 Relevant Planning History

- 3.1 A planning application (planning reference 19/00314/FUL) for the erection of a three storey detached building to accommodate six apartments (with two parking spaces) was refused permission by committee on 8th October 2019, for the following reason:

The proposal is considered to be an over-intensive form of development due to the small size of the site. In addition, it would create unacceptable parking problems due to the insufficient provision of on-site parking, resulting in a detrimental impact on neighbour amenity. Accordingly, the development would be contrary to Policy T11 of the Broxtowe Local Plan (2004), Policy 10 of the Broxtowe Aligned Core Strategy (2014) and Policy 17 of the Draft Part 2 Local Plan (2018).

- 3.2 An appeal against the refusal of the above planning application has been submitted to the Planning Inspectorate and is currently awaiting a start date.

4 Relevant Policies and Guidance

4.1 **Greater Nottingham Aligned Core Strategies Part 1 Local Plan 2014:**

- 4.1.1 The Council adopted the Core Strategy (CS) on 17 September 2014.

- Policy A: Presumption in Favour of Sustainable Development
- Policy 1: Climate Change
- Policy 2: The Spatial Strategy
- Policy 8: Housing Size, Mix and Choice
- Policy 10: Design and Enhancing Local Identity
- Policy 14: Managing Travel Demand

4.2 **Part 2 Local Plan**

- 4.2.1 The Council adopted the Part 2 Local Plan on 16 October 2019.

- Policy 1: Flood Risk

- Policy 15: Housing Size, Mix and Choice
- Policy 17: Place-making, design and amenity
- Policy 19: Pollution, Hazardous Substances and Ground Conditions

4.3 National Planning Policy Framework (NPPF) 2019:

- Section 2 – Achieving Sustainable Development.
- Section 4 – Decision-making.
- Section 5 – Delivering a sufficient supply of homes.
- Section 11 – Making efficient use of land.
- Section 12 – Achieving well-designed places.
- Section 14 – Meeting the challenge of climate change, flooding and coastal change.

5 Consultations

5.1 Nottinghamshire County Council as Highway Authority: The proposal for four apartments, with three parking spaces, complies with the requirement for unallocated parking. As such there are no concerns subject to conditions in regard to the provision of the access, the dropped kerb being widened, and the cycle spaces, parking and turning areas being available for use and surfaced in a bound material prior to the occupation of the apartments. The Highway Authority have commented on the amended plans, which now include four parking spaces, and have no objections subject to the conditions previously recommended.

5.2 Council's Environmental Health Officer: No objections, subject to a condition requiring glazing and ventilation to be installed in accordance with the approved noise assessment report, and to a note to applicant in regard to hours of noisy works and to no bonfires on site at any time.

5.3 Environment Agency: No objections subject to the development being carried out in accordance with the submitted Flood Risk Assessment (FRA). This will be secured by condition.

5.4 Waste and Recycling Officer: Sets out the requirements for the amount of bins and requirement for the bins to be presented at the edge of the adopted highway on collection day. The proposed layout shows a bin store area of a sufficient size to accommodate the required amount of waste storage containers.

5.5 NET: The application site is over 300m away from the NET tram tracks and therefore there are no comments or concerns.

5.6 22 properties were consulted and a site notice was displayed. 22 responses were received, 19 objecting on the following grounds:

- Three parking spaces for four apartments is insufficient, the proposal could house up to 14 people who would each have a car. The increase in vehicles would have a detrimental effect on the amenity of the local residents by increasing the use of the already overstretched parking along the nearby roads and pavements.

- Day time parking is mainly people working in the local area, evening parking is the residents of Queens Road and Station Road. Both cause problems for emergency services vehicles and refuse vehicles.
- Vehicles would have to reverse out of the site, at a risk to pedestrians, cyclists and other road users. A turning area should be provided within the site so that drivers can exit in a forward gear.
- The County Council's recent decision to refuse a residents parking permit scheme on Waverley Avenue will put more pressure upon on street parking particularly in the vicinity of the junction of Queens Road and Waverley Avenue.
- The design of the flats is not in keeping with the local character, as the modern design does not blend in with the local housing.
- During the recent wet weather, the sewerage system as existing could not cope. The development will exacerbate the issue.
- Queried whether highway land has been included within the site.
- No. 4 Waverley Avenue would lose privacy as a number of windows in the rear elevation overlook that garden.
- Over-intensive and out of keeping for the area and location. Will dominate the junction.
- Feels underhanded that the application has been put in twice without amendments.
- Family housing with parking and outside space for each property would be more appropriate.
- The building will overcrowd and overshadow the neighbouring properties and would cause overlooking.
- Access for emergency vehicles and refuse lorries would be compounded if the proposal were to be granted.
- The amount of air pollution given out at the traffic junction must be at a dangerous level, particularly during peak times. Surprised that a residential development of this intensity is considered suitable.

5.6.1 Three letters of observation made the following points:

- The size of the second double bedroom is too small to enable someone to walk round the double bed and would not be classed a double. The design should be changed to a single if people are not going to be crammed in. The scheme would then look like a development for professional people rather than another HMO.
- If the ground floor two bedroom apartment is changed to a one bed, the space gained can be used for the bin store and cycle rack, enabling a fourth parking bay to be provided, one space per apartment.
- Consideration could be made to placing a restriction on any occupier of the proposed development not being able to join any local parking scheme. This encourages people to use public transport and not add to the problem of over parking.

5.6.2 A re-consultation on the amended plans has been carried out and responses received will be reported as late items.

6 Assessment

6.1 The main issues for consideration are impact on the amenities of the occupiers of neighbouring property; impact on highway safety and parking; flood risk; and amenity of the future occupiers of the apartments and whether previous reasons for refusal have been addressed.

6.2 **Principle**

6.2.1 The site is not allocated for any specific purpose within the Broxtowe Local Plan. Whilst there is a mix of uses in the area, the immediate surrounds to the north, north west and south west is one of a residential character. The principle of residential development on this site is acceptable, subject to the considerations set out below.

6.2.2 The proposal for the two storey building follows on from the consideration of the previous planning application, which was for a three storey building of six apartments. This previous scheme was refused at October committee as it was considered the development would be over-intensive (due to the small size of the site) and would create unacceptable parking problems due to the insufficient provision of on-site parking. It is considered that the revised application, which is the subject of this report, addresses the concerns raised in that the density of development has been reduced, from six apartments to four, and the amount of parking spaces has increased from two to four, thereby increasing the apartment to parking ratio by providing one parking space per apartment, thereby minimising any potential impact in regard to on-street parking.

6.3 **Amenity**

6.3.1 The rear elevation of the proposed building, which faces both 4 Waverley Avenue and 198 Station Road, would have three windows at ground floor level, which would face these two properties, and three windows at first floor level. Two of the first floor windows would be oriel windows, which are angled so as to face toward Waverley Avenue, and the third is shown to have obscure glazing. There are a further two small windows (one to each floor) within the side elevations directly facing 198 Station Road. These windows are shown to be obscurely glazed and serve en-suite bathrooms. As such it is considered that the development would not result in a loss of privacy for the occupiers of these two properties. In regard to outlook and loss of light, it is considered that the building, being positioned to the east and south east of the site, would not result in a significant loss of light for these occupiers, and in terms of outlook, there are no facing windows in the side elevation of 4 Waverley Avenue. It is considered that views from the windows in the rear wing at 198 Station Road would not be significantly affected.

6.3.2 286 Queens Road, to the opposite side of Waverley Avenue, has windows in the side elevation facing the site. Whilst there are windows proposed in the side elevation of the proposed building, and the oriel windows face toward Waverley Avenue, it is considered that due to the distance between, over the public highway, the development would not have a significant impact on the amenities of the occupiers of this property.

- 6.3.3 Amended plans include an increase in the distance between the proposed building and the adjacent properties, as the footprint of the building has been reduced. There is an increase of 1m between the proposed building and 4 Waverley Avenue, and the north elevation has been set back from the common boundary with 198 Station Road, to allow for bin and cycle storage. The roof shape has also been altered, with a flat roof element introduced to the central part. These amendments would further minimise any impact that the building would have on the amenities of the occupiers of 4 Waverley Avenue and 198 Station Road.
- 6.3.4 Even with the slight reduction in floorspace, the internal layout would provide the future occupiers with an acceptable amount of living space, which complies with the Department for Communities and Local Government's Nationally Described Space Standards, and provide access to natural light and to an outlook. All four apartments would have Juliette balconies, to the front elevation. In regard to air pollution, it is acknowledged that the site is close to a busy signal controlled traffic junction and that at busy times of the day air pollution may be higher than normal. The apartments would need to comply with any requirements under the Building Regulations and as such the occupants would have access to adequate ventilation at times when they may choose to keep any windows and doors closed.
- 6.3.5 The site is located at a busy traffic controlled junction. A noise assessment has been submitted which includes recommendations for mitigation measures such as choice of building materials, glazing and ventilation requirements in order to ensure that the future occupants are protected from background noise arising from traffic standing at the junction, and general road noise. A condition to ensure that the development is carried out in accordance with the noise mitigation measures detailed in the noise assessment will be included as part of the decision.

6.4 Design, scale and appearance

- 6.4.1 The application site occupies a prominent position within the street scene and as such it is important that the scale and massing as well as the design and materials are of a high quality.
- 6.4.2 A two storey building is proposed, built slightly set back from the pavement. There are three storey buildings within the area, these being the apartment buildings to the south west of Queens Road, and there are tall Victorian two storey dwellings along the same side of Queens Road as the application site, as well as along Station Road, directly adjacent to the site. The proposed building has two pitched roofs, with a flat roof between, inset from the building edge. The scale of the building is similar to other buildings in the immediate area. A full height glazed entrance, with contrasting cladded panels, is shown to the front elevation and it is considered that this would help to minimise any impact in terms of massing by providing a visual break along the frontage. It is considered that the density of the site is appropriate and is in keeping with the character of the area.
- 6.4.3 The materials proposed are to be a mix of red brick, grey fibre cement cladding panels, grey fibre cement hanging slates, grey powder coated aluminium window and door frames, and grey fibre cement slates to the roof. These materials reflect the palette of the surrounding buildings and are considered acceptable in principle. Details of materials will be secured by condition.

6.4.4 There would be a brick boundary wall with railings along the street frontage, to Queens Road / Station Road, at a height of 1.8m. The bin and cycle store is adjacent to the north elevation, and would be accessed from both Station Road and from the parking area. Landscaped areas would be provided within the site, to the front behind the boundary wall, and to the rear.

6.5 Access and Parking

6.5.1 Vehicular access into the site will utilise the existing access from Waverley Avenue. This is considered to be acceptable due to the site's location at a junction. A pedestrian gate would be provided off Station Road, giving access to the bin and cycle storage and to and from the parking area.

6.5.2 Access into the building is via a stepped entrance to the front elevation. The ground floor level is raised in order to comply with flood risk requirements. Whilst the entrance would not be fully accessible by all potential occupiers, it is considered that due to the site's location within Flood Zone 3, a stepped entrance would be acceptable. Access into the building would need to comply with Building Regulations.

6.5.3 Four parking spaces and four cycle spaces are proposed within the site.

6.5.4 It is clear from the consultation responses to both this and the previous planning application that there is significant concern that the development does not include sufficient parking provision within the site and that this would lead to increased demand for on-street parking. Concerns are also expressed in regard to existing congestion along Waverley Avenue and adjacent streets.

6.5.5 In regard to assessing the highway impacts of a proposal, paragraph 109 of the NPPF states that development should only be refused on transport grounds where the residual cumulative impacts are severe. Whilst paragraph 105 refers to the setting of local parking standards rather than the determination of applications, it provides a list of factors which should be taken into account, including the availability of and opportunities for public transport and the type, mix and use of the development. Policy 10 of the Aligned Core Strategy states that development should be designed to reduce the dominance of motor vehicles.

6.5.6 The site lies within close proximity to both Beeston town centre (10 minutes walk) and to the railway station (5 minutes walk). Bus services also run along Queens Road and bus stops are close by. The parking spaces are of sufficient dimensions. It is accepted that there may be additional parking demand from the development and this may lead to on-street parking along Waverley Avenue. However, it is considered that there would not be a severe highways impact and residents would have the opportunity to use more sustainable transport options. Furthermore, the Highways Authority states no objection and it is considered that a pragmatic approach also needs to be taken in respect of developing sites within existing urban areas. Based on the above, it is considered that there would not be sufficient policy justification for refusing the application on transport or parking grounds.

6.5.7 A condition is required to ensure that the dropped kerb is extended and the parking area suitably surfaced and drained, and available for use, along with the cycle

parking, prior to the occupation of the apartments, and that the access is of a sufficient width.

6.6 Flood Risk

6.6.1 The site is within Flood Zones 2 and 3 which is land with a high probability (1 in 100 or greater) of river flooding. A Flood Risk Assessment (FRA) has been submitted with the application. Paragraphs 155 – 158 of the NPPF state that inappropriate development in areas of high risk of flooding should be avoided but where it is necessary, should be undertaken without increasing flood risk elsewhere. All plans should apply a sequential, risk-based approach to the location of development in order to steer new development to areas with the lowest risk of flooding. A Sequential Test has also been submitted with the application which concludes that there are no alternative sites available within areas located in zones with a lower probability of flooding. The Environment Agency has raised no objections subject to the development being carried out in accordance with the submitted FRA, which includes the elevated internal floor levels; the use of flood resistant external materials; high level utility services and sockets; and non-return valves fitted to drains. An appropriate drainage system would be included. It is considered that the proposal would satisfactorily mitigate for flood risk.

6.7 Other Matters

6.7.1 In regard to the sewerage system, this is not considered to be a material planning matter and would be one which would be covered under building regulations.

6.7.2 The application site boundary, which is shown to extend into the footpath, is annotated as being within the ownership of the site owner. Notwithstanding this, no part of the built form is shown to encroach on this land and as such the ownership or otherwise would not have an impact on the assessment of the application.

6.7.3 The comment that the application has been put in twice with no amendments is incorrect, as the former application, reference 19/00314/FUL, proposed six apartments and two parking spaces whilst the current application, which is the subject of this report, is for four apartments with four parking spaces.

6.7.4 Family housing with parking and amenity space has been suggested as an alternative to the apartment scheme. Whilst a scheme of this nature could be acceptable in principle, the planning authority is statutorily obliged to consider the planning application as submitted. Should an application be submitted for an alternative scheme, this would be assessed separately.

6.7.5 It would not be considered reasonable or enforceable to place a restriction on occupiers of the proposed building from being able to join any local parking scheme. It is understood that the County Council have recently determined that they would not be implementing a residential parking permit scheme in this area, given that the occupiers of properties on Queens Road and Station Road are unable to park outside their properties and therefore are expected to park on the side streets, if required, which includes Waverley Avenue. Notwithstanding this, should there be a scheme in the future, each property would need to apply or comply in accordance with the requirements set down in that scheme.

6.7.6 The plans have been amended to include a further reduction in the ground floor footprint, to facilitate the accommodation of an additional parking space.

6.7.7 Whilst double beds are shown in each bedroom, these are for illustrative purposes only and would not determine the actual level of occupation. Additionally, the type of tenancy (e.g. professionals or non-professional) cannot be considered as a material planning matter. The proposed sizes of the apartments, which range from 46.4 square metres for a one-bed and between 62.8 and 72.6 square metres for a two-bed, are in excess of the internal floor areas of a minimum 39 square metres, for a one-bed, and a minimum of 61 square metres, for a two bed, set out in the Department for Communities and Local Government Technical Housing Standards (nationally described space standard) and as such are considered to be able to provide an adequate level of internal space, with access to natural light and to an outlook for all habitable rooms.

7 Planning Balance

7.1 The benefits of the proposal are that it would see the redevelopment of a vacant site, would provide housing in an existing urban area and would be in accordance with the policies contained within the development plan. This is given significant weight.

7.2 The negative impact would be the potential for an increase in on-street parking in the area.

8 Conclusion

8.1 The proposed development of four apartments is considered to be acceptable and would not be harmful to the character or appearance of the area. The proposal also gives an opportunity to return the site into use and provide housing in a sustainable location. The on-site parking provision ratio has been increased (to 100 per cent), and a covered cycle store is to be provided, which satisfactorily reduces the potential for an increase in on-street parking. Residential amenity will not be unduly affected by the proposals.

<u>Recommendation</u>	
The Committee is asked to RESOLVE that planning permission be granted subject to the following conditions.	
1.	The development hereby permitted shall be commenced before the expiration of three years beginning with the date of this permission. <i>Reason: To comply with S91 of the Town and Country Planning Act 1990 as amended by S51 of the Planning and Compulsory Purchase Act 2004.</i>
2.	The development hereby permitted shall be carried out in accordance with the Site Location Plan received by the Local

	<p>Planning Authority on 3 September 2019, and drawings numbered 2598(08)A11 rev A, 2598(08)A12 rev B, 2598(08)A13 rev A, 2598(08)A14 rev A, 2598(08)902 rev A, 2598(08)202 rev A, 2598(08)E12 rev A, 2598(08)E11 rev A, 2598(08)G02 rev B 2598(08)102 rev A and 2598(08)S11 rev A received by the Local Planning Authority on 22 November 2019.</p> <p><i>Reason: For the avoidance of doubt.</i></p>
<p>3.</p>	<p>No above ground works shall be carried out until details of the manufacturer, type and colour of the bricks, tiles and cladding to be used in facing elevations have been submitted to and approved in writing by the Local Planning Authority, and the development shall be constructed only in accordance with those details.</p> <p><i>Reason: Limited details were submitted and to ensure the development presents a satisfactory standard of external appearance, in accordance with the aims of Policy 17 of the Broxtowe Part 2 Local Plan (2019) and Policy 10 of the Aligned Core Strategy (2014).</i></p>
<p>4.</p>	<p>No above ground development shall take place until a landscaping scheme has been submitted to and approved in writing by the Local Planning Authority. This scheme shall include the following details:</p> <ul style="list-style-type: none"> (a) numbers, types, sizes and positions of proposed shrubs (b) proposed hard surfacing treatment (c) planting, seeding/turfing of other soft landscape areas. <p>The approved scheme shall be carried out strictly in accordance with the agreed details.</p> <p><i>Reason: Limited details were submitted and to ensure that the details are satisfactory in the interests of the appearance of the area and in accordance with the aims of Policy 17 of the Broxtowe Part 2 Local Plan (2019) and Policy 10 of the Aligned Core Strategy (2014).</i></p>
<p>5.</p>	<p>No above ground development shall take place until details of the bin store and a covered cycle store have been submitted to and approved in writing by the Local Planning Authority.</p> <p><i>Reason: Limited details were submitted and to ensure the development presents a satisfactory standard of external appearance, in accordance with the aims of Policy 17 of the Broxtowe Part 2 Local Plan (2019) and Policy 10 of the Aligned Core Strategy (2014).</i></p>
<p>6.</p>	<p>No part of the development shall be occupied until the vehicular access has been widened, made available for use and constructed</p>

	<p>in accordance with the Highway Authority specification. The access shall thereafter be retained for the lifetime of the development.</p> <p><i>Reason: In the interests of highway safety in accordance with the aims of Policy 10 of the Aligned Core Strategy (2014).</i></p>
7.	<p>No part of the development hereby permitted shall be brought into use until the parking and turning areas are surfaced in a bound material with the parking bays clearly delineated in accordance with drawing number 2598 (08) G02 rev B. The parking and turning areas shall be maintained in the bound material for the life of the development and shall not be used for any purpose other than the parking and turning of vehicles.</p> <p><i>Reason: In the interests of highway safety in accordance with the aims of Policy 10 of the Aligned Core Strategy (2014).</i></p>
8.	<p>No part of the development hereby permitted shall be brought into use until the cycle parking as indicated on drawing 2598 (08) G02 rev B has been provided and that area shall not thereafter be used for any purpose other than the parking of cycles.</p> <p><i>Reason: In the interests of promoting sustainable modes of transport, in accordance with the aims of Policy 10 of the Aligned Core Strategy (2014).</i></p>
9.	<p>The approved landscaping shall be carried out not later than the first planting season following the substantial completion of the development or occupation of the building, whichever is the sooner and any trees or plants which, within a period of 5 years, die, are removed or have become seriously damaged or diseased shall be replaced in the next planting season with ones of similar size and species to the satisfaction of the Local Planning Authority, unless written consent has been obtained from the Local Planning Authority for a variation.</p> <p><i>Reason: To ensure the development presents a more pleasant appearance in the locality and in accordance with Policy 17 of the Broxtowe Part 2 Local Plan (2019) and Policy 10 of the Aligned Core Strategy (2014).</i></p>
10.	<p>The development shall be carried out in accordance with the submitted flood risk assessment (reference FRA-MA10992-R01) and finished floor levels shall be set no lower than 27.84m above Ordnance Datum (AOD) and flood resilience construction measures shall be incorporated throughout the development as stated within.</p> <p>These mitigation measures shall be fully implemented prior to</p>

	<p>occupation and shall be retained and maintained thereafter throughout the lifetime of the development.</p> <p><i>Reason: To reduce the risk of flooding to the proposed development and future occupants, in accordance with the aims of Policy 1 of the Aligned Core Strategy (2014).</i></p>
11.	<p>The windows on the first floor rear elevation and at ground and first floor level on the side (north) elevation annotated as privacy glass on drawing nos. 2598(08)102 rev A and 2598(08)G02 rev B shall be obscurely glazed to Pilkington Level 4 or 5 (or such equivalent glazing which shall first have been agreed in writing by the Local Planning Authority) and retained in this form for the lifetime of the development.</p> <p><i>Reason: In the interests of privacy and amenity for nearby residents and in accordance with the aims of Policy 17 of the Broxtowe Part 2 Local Plan (2019) and Policy 10 of the Aligned Core Strategy (2014).</i></p>
12.	<p>The development shall be carried out in accordance with the noise mitigation measures as detailed in the Acute Acoustics Ltd Noise Assessment report, reference 2347 Beeston-Waverley Avenue, dated 18th March 2019.</p> <p><i>Reason: In the interest of the amenities of the future residents and in accordance with the aims of Policy 19 of the Broxtowe Part 2 Local Plan (2019).</i></p>
	<p>NOTES TO APPLICANT</p>
1.	<p>The Council has acted positively and proactively in the determination of this application by working to determine it within the agreed determination timescale.</p>
2.	<p>The proposed development lies within a coal mining area which may contain unrecorded coal mining related hazards. If any coal mining feature is encountered during development, this should be reported immediately to the Coal Authority on 0345 762 6848.</p> <p>Further information is also available on the Coal Authority website at: www.gov.uk/government/organisations/the-coal-authority</p>
3.	<p>The proposal makes it necessary to widen the vehicular crossing over a footway of the public highway. These works shall be constructed to the satisfaction of the Highway Authority. You are, therefore, required to contact the County Council's Customer Services on telephone 0300 500 80 80 to arrange for these works to be carried out.</p>

4.	Given the proximity of residential properties, it is advised that contractors limit noisy works to between 08.00 and 18.00 hours Monday to Friday, 08.00 and 13.00 hours on Saturdays and no noisy works on Sundays and Bank Holidays. There should also be no bonfires on site at any time.
5.	No planting shall take place within the limits of the adopted highway.
6.	No windows or doors at ground floor level shall overhang the adopted highway.



Legend

-  Site
-  Flood Zone 3
-  Flood Zone 2

Photographs



Across the site to 4 Waverley Avenue (left) and 198 Station Road (right)



View looking west, from the junction of Station Road and Queens Road



286 Queens Road, to the south west of the site and at the corner of Waverley Avenue



Waverley Avenue, taken mid-morning on Monday 23rd September

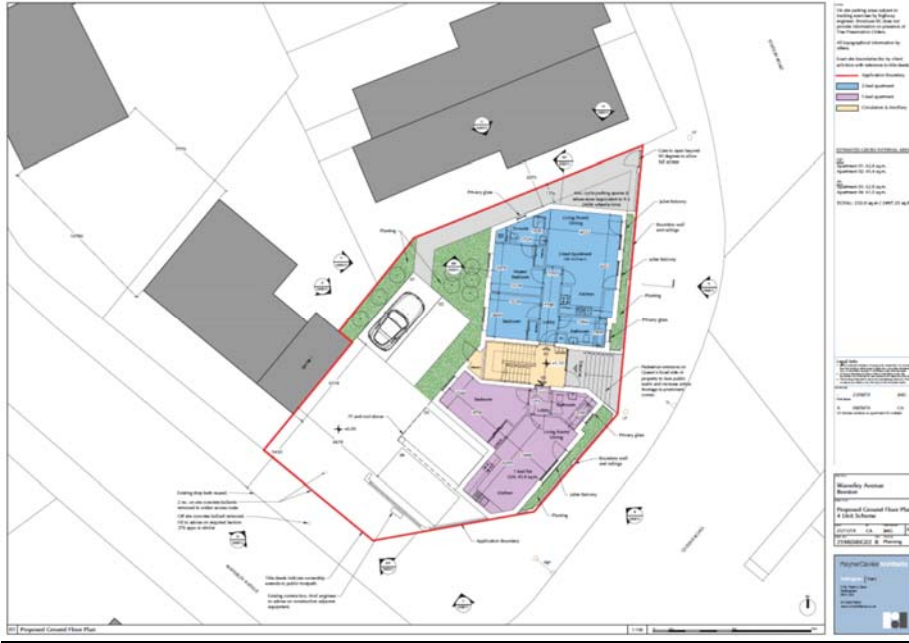


Co-op store to the south east opposite the site



Hoardings to the north east of the site, on the corner of Queens Road and Station Road

Plans (not to scale)



Ground floor layout



First floor layout



Proposed front elevations



Proposed rear and side elevations

Report of the Chief Executive

APPLICATION NUMBER:	19/00600/FUL
LOCATION:	LAND ON THE NORTH WEST SIDE OF THE MAN IN SPACE, NOTTINGHAM ROAD, EASTWOOD, NOTTINGHAMSHIRE
PROPOSAL:	CONSTRUCT THREE STOREY APARTMENT BUILDING CONTAINING 8 APARTMENTS, INCLUDING 8 CAR PARKING SPACES

The application has been called in to Committee by Cllr M Radulovic MBE.

1 Executive Summary

- 1.1 This application seeks permission to construct an 8 unit apartment block, including a car park with 8 parking spaces. The proposed development will be 3 storeys and front on to Nottingham Road. All apartments will have 1 bedroom and will be approximately 40 – 41 sq.m in size.
- 1.2 The proposed development is between a Sainsbury local and a café and takeaway with residential flats above. There are residential dwellings fronting on to Cross Street to the rear of the site and a mix of uses to the north of Nottingham Road. It is considered that the proposal will not result in an unacceptable loss of amenity for any neighbouring residents.
- 1.3 There are a mix of property styles and sizes in the surrounding area and it is considered that the proposal will not be out of keeping with the character of the area. The proposed development will follow the building line of the neighbouring property to the north west with adequate separation and a change in roof height to ensure it will not result in a terraced effect that would be harmful to the street scene.
- 1.4 The parking and access arrangements to the rear of the proposed development are considered proportionate to the scheme and therefore acceptable. It is considered that the proposal will not result in an unacceptable impact on highway safety.
- 1.5 The proposal will result in the re-development of a vacant brownfield site in a built up area, contributing to the Council's housing supply in a sustainable location. The development will have a modern but simple design that is not harmful to the street scene or out of keeping with the character of the area. Whilst the development may result in some loss of light to the flats to the north west of the site, it is considered that the positive impacts of the development on balance outweigh any potential harm.
- 1.6 Overall it is considered that the scheme is acceptable and it is therefore recommended planning permission is granted in accordance with the resolution contained within the appendix.

1 Details of the Application

- 1.1 This application seeks permission to construct a 3 storey, 8 unit apartment block. The apartment block will front on to Nottingham Road, with the application site covering the land between Nottingham Road and Cross Street to the rear. Vehicular access to the site will be provided via Cross Street, with parking for 8 cars being to the rear of the proposed apartment block. The proposal has a contemporary design with a flat roof and a mix of render and brickwork finishes.

2 Site and surroundings

- 2.1 The proposed development will be situated on a vacant site between Nottingham Road and Cross Street. The land slopes away from the north east to the south west of the site. There is a mix of retail units and residential properties in the surrounding area including a Sainsbury local and vets with associated car parks immediately to the south east of the site and a café and takeaways with flats above immediately to the north west. Residential dwellings are situated opposite the proposed access to the site on Cross Street and two dwellings have recently been constructed immediately to the south west.

3 Relevant Planning History

- 3.1 Outline planning permission 14/00437/OUT was granted in 2014 to construct a ground floor retail unit with 4 No. residential flats above. An application for reserved matters for this scheme was never submitted and the outline permission has therefore now expired.

4 Relevant Policies and Guidance

4.1 **Greater Nottingham Aligned Core Strategies Part 1 Local Plan 2014:**

- 4.1.1 The Council adopted the Core Strategy (CS) on 17 September 2014.

- Policy A: Presumption in Favour of Sustainable Development
- Policy 2: The Spatial Strategy
- Policy 8: Housing Mix and Choice
- Policy 10: Design and Enhancing Local Identity

4.2 **Part 2 Local Plan 2019**

- 4.2.1 The Council adopted the Part 2 Local Plan on 16 October 2019.

- Policy 15: Housing size, mix and choice
- Policy 17: Place-making, design and amenity
- Policy 20: Air Quality

4.3 **National Planning Policy Framework (NPPF) 2019:**

- Section 2 – Achieving Sustainable Development.

- Section 4 – Decision-making.
- Section 12 – Achieving well-designed places.

5 Consultations

- 5.1 **Council's Environmental Health Officer:** No objection raised.
- 5.2 **Highways Authority:** No objection. Has referred to the Highways Authority standing advice which advises that 1 parking space per 1 bedroom apartment is acceptable.
- 5.3 18 properties either adjoining or opposite the site were consulted and a site notice was displayed. 1 objection has been received on the grounds of traffic generation and access.
- 5.4 A 7 day re-consultation was carried out following the submission of amended plans and no additional comments from members of the public have been received.

6 Assessment

- 6.1 The main issues for consideration are the impact of the proposal on neighbouring amenity, design and appearance and the impact of the proposal on highway safety.
- 6.2 Amendments to the original proposal were sought including changes to the front elevation to break up the massing by stepping the front elevation, increasing the size of the openings and recessing the openings. The position of the bin store was also moved so it would not be in line with the rear garden of the neighbouring dwelling. Amended plans were submitted and a 7 day re-consultation was carried out.
- 6.2 **Principle**
- 6.2.1 The application site is not covered by any site specific planning policies. It is a vacant brownfield site in a built up area consisting of retail units, cafes and residential dwellings. It is therefore considered that the principle of residential development on this site is acceptable and would be in accordance with Policies 15 and 17 of the Local Plan Part 2.

6.3 **Amenity**

- 6.3.1 To the north west of the application site is a development which includes cafes and hot food takeaways at ground floor level with residential flats at first floor level. There are two small windows facing towards the application site at first floor level. The proposed development will be set in from this boundary by approximately 1.6m, with the neighbouring property set in from the boundary by approximately 1.2m. The application property will be slightly taller than the existing building to the north west. However, due to the size and position of the windows and the relationship with the application site, it is considered there will not be an unacceptable impact. It is also noted that these windows rely on light and outlook from outside of the ownership of that property. The furthest forward window of the neighbouring building is also a relatively small opening and therefore it is considered that the loss of light experienced by this property will be minimal.

- 6.3.2 There are two openings proposed on each level of the apartment block on the north west side. Whilst these openings face towards the neighbouring property to the north west, these windows will serve the staircase and not a primary living area. Provided the windows are obscurely glazed it is considered that the proposal will not result in an unacceptable loss of amenity for the residents of the flats to the north west of the site.
- 6.3.3 The rear elevation of the proposed apartment block will be approximately 21.8m from the south west boundary of the site, adjoining Cross Street. There are two relatively new dwellings that adjoin the site and are located on Cross Street. These dwellings face on to Cross Street, with their rear gardens being towards to the proposed apartment block. There will be a separation distance of approximately 14m between the rear elevation of these dwellings and the rear elevation of the proposed apartment block, which is considered sufficient to ensure it will not result in an unacceptable sense of enclosure for the residents of these dwellings.
- 6.3.4 There will be openings on the rear elevation of the proposed apartment block and these may result in some overlooking of the rear gardens of the two dwellings on Cross Street. However, the proposed apartment block is not directly behind these dwellings, and the angle is considered sufficient to avoid an impression of direct overlooking from the proposed apartment block. It is therefore considered the proposal will not result in an unacceptable loss of privacy for the residents of the dwellings.
- 6.3.5 The bin store will be in line with the side elevation of the neighbouring property on Cross Street and it is noted that a new retaining wall with 2m high timber fence will be installed along the boundary. The bin store will not go beyond the rear elevation of the neighbouring property which in combination with the boundary treatment, is considered to be sufficient to ensure the bin store will not result in an unacceptable impact on the neighbouring residents. It is considered appropriate to condition that the boundary wall and fence is built prior to the first use of the development and for details of the bin store to be submitted prior to occupation.
- 6.3.6 Other nearby properties on Cross Street are on the south side of the road, facing towards the application property. The rear elevation of the development will be approximately 32m from the front of these dwellings which, despite the change in level, is considered a sufficient distance to ensure it will not result in an unacceptable loss of amenity for any of these properties.
- 6.3.7 To the east/south east the application site adjoins the site for Sainsbury Local and vets and the associated car park. Due to the nature of these uses and the intervening car park, the proposal will not result in any loss of amenity to this side.
- 6.3.8 The apartment block will be set back from Nottingham Road by approximately 6m, giving a separation distance of approximately 25m from the front elevation of the properties on the north side of the road. It is considered that this separation is sufficient to ensure that the proposal will not result in an unacceptable loss of amenity for the neighbouring properties to the north of Nottingham Road.
- 6.3.9 The proposed apartments will all be either 40 or 41 sq.m in size and will each have one bedroom, an open plan living room and kitchen and a shower room. It is

considered that the principal living rooms in each apartment will have sufficient natural light and ventilation and will have open outlooks. Overall it is considered that the future occupiers of the proposed apartments will have a satisfactory level of amenity.

- 6.3.10 The site is not in an Air Quality Management Area. In addition, the approval of residential developments that are within walking distance of bus provision is considered to have a positive benefit on wider air quality issues by encouraging public transport use and therefore reducing the need to travel by private car.
- 6.3.11 Overall it is considered that the proposed apartment block will not result in an unacceptable loss of amenity for any neighbouring properties and that the future occupiers of the development will have a satisfactory level of amenity in accordance with Policy 17 of the Broxtowe Part 2 Local Plan.

6.4 Design and Appearance

- 6.4.1 The proposed design is for a three storey apartment block that fronts on to Nottingham Road. It is set back from Nottingham Road by approximately 6m, following the building line of the neighbouring property to the north west. It will have a reasonable separation distance from the neighbouring property to the north west and the slightly taller block will help to break up the street scene without resulting in an overly dominant development. The change in height and clear difference in style between the application property and the properties on either side ensures it will not result in a terraced effect that would be harmful to the street scene.
- 6.4.2 Whilst the change in land level from Cross Street to Nottingham Road will give the apartment block a more prominent appearance from Cross Street, it is considered that it is set far enough into the site to ensure this will not be overly dominant in the street scene to the rear.
- 6.4.3 The proposed development has a modern design with a flat roof and mix of render and brick finish on the facing elevations. The front elevation is slightly staggered and the openings are recessed which works to break up the elevation whilst retaining a simplicity to the design. The front elevation has been designed with large openings that are in proportion with the scale of the front elevation.
- 6.4.4 This section of Nottingham Road has a range of building types with more traditional properties to the north side of the road, and a mix of more modern buildings along the south side of the road. There is a mix of materials throughout the area including traditional red brick, lighter bricks and render. Taking the mix of property types and scales in the surrounding area into account it is considered that the proposed apartment building will not be out of keeping with the character of the area.
- 6.4.5 There are two street trees beyond the frontage that will not be affected by the development.
- 6.4.6 Overall it is considered that a satisfactory standard of design has been achieved.

6.5 Access

- 6.5.1 The Highways Authority has not raised any objection to the proposal, stating that their standing guidance should be taken into account when determining the application.
- 6.5.2 The proposal includes a car park to the rear of the development, with parking provided for 8 cars which is considered sufficient to ensure the proposal will not result in a significant increase in on street parking in the surrounding area. The access to the site will be approximately 5m wide which is sufficient for the proposed number of apartments. Access to the car park will be off Cross Street, which is a relatively short stretch of road with a 30mph limit. The access is not in close proximity to the junction and taking into account the speed restriction of the road it is considered that safe access and egress to the site can be achieved.
- 6.5.3 The addition of 8, 1 bedroom apartments is not considered to result in an unacceptable amount of additional traffic in the surrounding area. Furthermore, the development is in close proximity to bus stops on Nottingham Road as well as a number of local amenities such as shops, which will reduce reliance on cars for local use.
- 6.5.4 Overall it is considered that the proposal will not result in any unacceptable highway safety issues.

7 Planning Balance

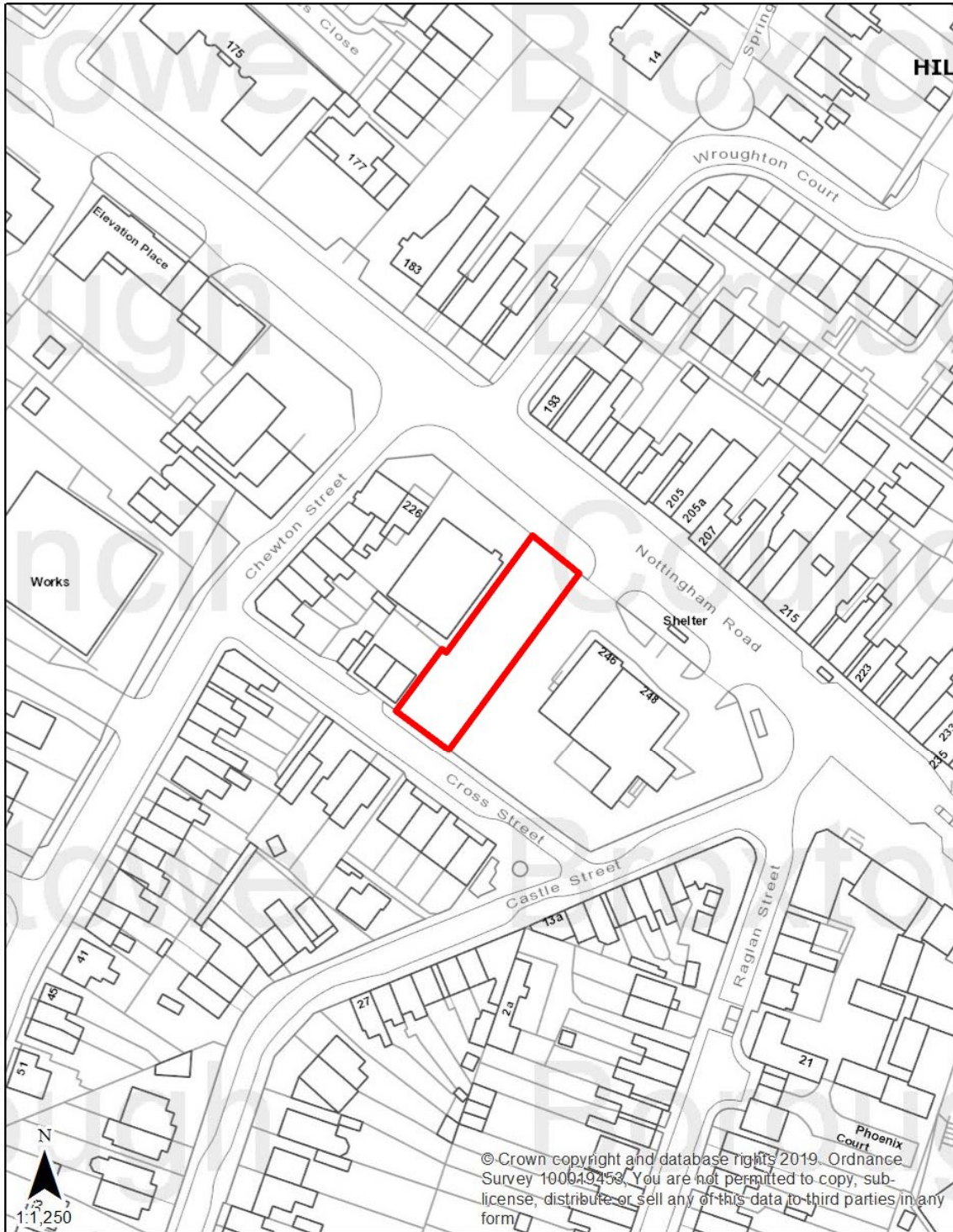
- 7.1 The benefits of the proposal are that it will result in the re-development of a vacant brownfield site in a built up area, contributing to the Council's housing supply in a sustainable location. The development will have a modern but simple design that is not harmful to the street scene or out of keeping with the character of the area.
- 7.2 Whilst the development may result in some impact on the flats to the north west of the site, it is considered that the impact would not be unacceptable and the positive impacts of the development notably the use of a brownfield site to increase the Council's housing supply, on balance outweigh any potential harm.

8 Conclusion

- 8.1 To conclude, it is considered that the proposal has been designed to a satisfactory standard that responds well to the street scene and the character of the surrounding area. It is therefore considered that the scheme is acceptable and planning permission should be granted.

<u>Recommendation</u>	
<p>The Committee is asked to RESOLVE that planning permission be granted subject to the following conditions.</p>	
1.	<p>The development hereby permitted shall be commenced before the expiration of three years beginning with the date of this permission.</p> <p><i>Reason: To comply with S91 of the Town and Country Planning Act 1990 as amended by S51 of the Planning and Compulsory Purchase Act 2004.</i></p>
2.	<p>The development hereby permitted shall be carried out in accordance with the drawings numbered ST-347/02 revision B (1:100), ST-347/03 revision C (1:100), ST-347/01 revision B (1:200, 1:100); received by the Local Planning Authority on 18 September and 23 October 2019.</p> <p><i>Reason: For the avoidance of doubt.</i></p>
3.	<p>No part of the development hereby permitted shall be brought into use until the dropped vehicular footway and parking spaces as shown on drawing ST-347/02 Rev B, are available for use and constructed in accordance with the Highway Authority specification.</p> <p><i>Reason: In the interests of Highway safety.</i></p>
4.	<p>No part of the development hereby permitted shall be brought into use until the parking, turning and servicing areas are surfaced in a bound material and with provision to prevent the unregulated discharge of surface water from the driveway/parking/turning areas to the public highway. The parking, turning and servicing areas shall be maintained in the bound material for the life of the development.</p> <p><i>Reason: To reduce the possibility of deleterious material being deposited on the public highway (loose stones etc.) and to ensure surface water from the site is not deposited on the public highway causing dangers to road users.</i></p>
5.	<p>No part of the development hereby permitted shall be brought into use until the boundary wall and fence on the north west boundary of the site is erected in accordance with the details on drawing no. ST-347/01 revision B.</p> <p><i>Reason: To protect the amenity of the neighbouring residents and in accordance with Policy 17 of the Broxtowe Part 2 Local Plan (2019).</i></p>
6.	<p>The development hereby permitted shall not be first occupied until details of the bin store have been submitted to and approved in</p>

	<p>writing by the Local Planning Authority. The bin store shall then be erected in accordance with these details.</p> <p><i>Reason: To ensure a satisfactory standard of appearance and in accordance with Policy 17 of the Broxtowe Part 2 Local Plan (2019).</i></p>
7.	<p>The windows above ground floor level in the north west elevation of the development hereby permitted shall be obscurely glazed to Pilkington level 4 or 5 and retained as such for the lifetime of the development.</p> <p><i>Reason: In the interest of protecting the amenity of the residents in the neighbouring properties and in accordance with Policy 17 of the Broxtowe Part 2 Local Plan (2019).</i></p>
8.	<p>No above ground works shall take place until details including the colour and manufacturer of the brick and render have been submitted to and approved in writing by the Local Planning Authority. The development shall then be carried out in accordance with these details.</p> <p><i>Reason: To ensure a satisfactory standard of appearance and in accordance with Policy 17 of the Broxtowe Part 2 Local Plan (2019).</i></p>
	<p>NOTES TO APPLICANT</p>
1.	<p>The Council has acted positively and proactively in the determination of this application by working to determine it within the eight agreed determination timescale.</p>
2.	<p>The proposed development lies within a coal mining area which may contain unrecorded coal mining related hazards. If any coal mining feature is encountered during development, this should be reported immediately to the Coal Authority on 0345 762 6848.</p> <p>Further information is also available on the Coal Authority website at: www.gov.uk/government/organisations/the-coal-authority</p>
3.	<p>The development makes it necessary to construct a vehicular crossing over a footway of the public highway. You are therefore required to contact the County Council's Highways Area Office tel. 0300 500 80 80 to arrange for these works to be carried out.</p>



Legend

 Site

Photographs



Application site from Nottingham Road.



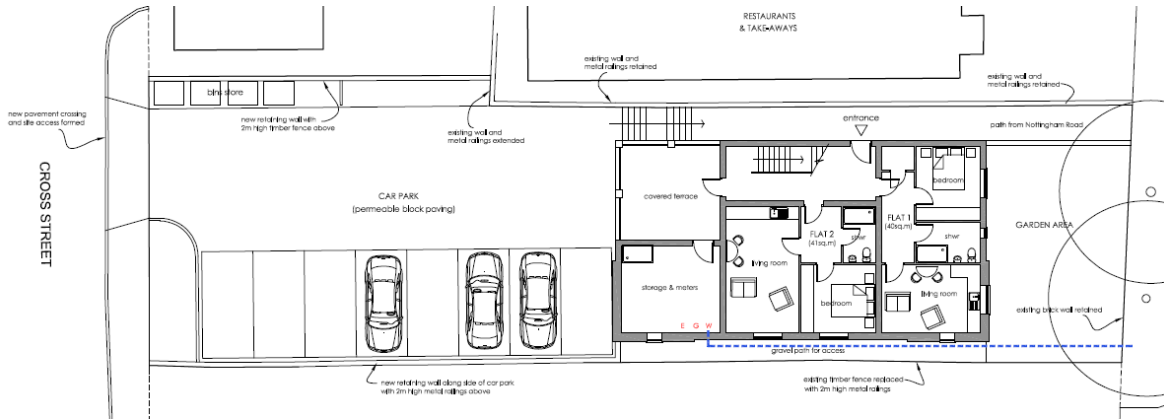
Application site from Nottingham Road.



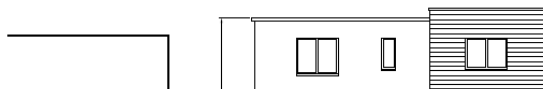
Application site from Cross Street.



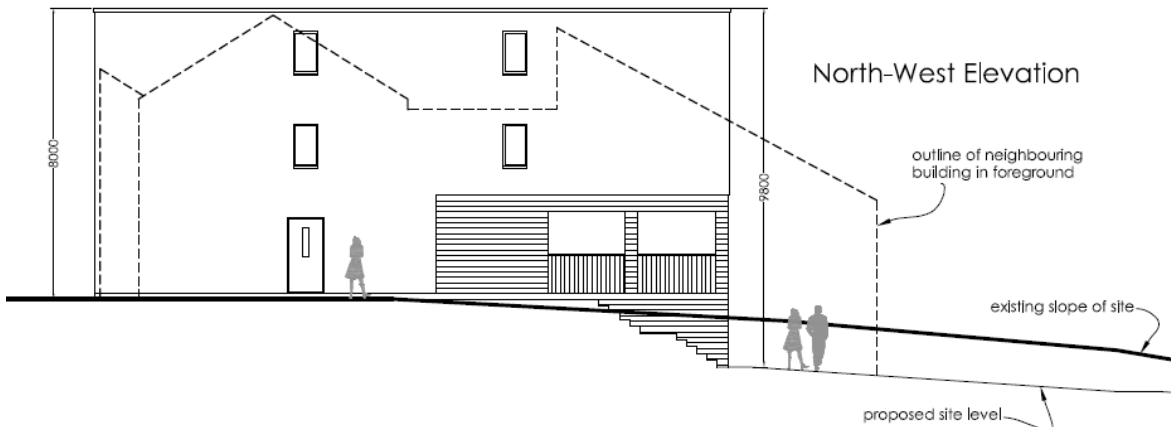
Application site from Cross Street.

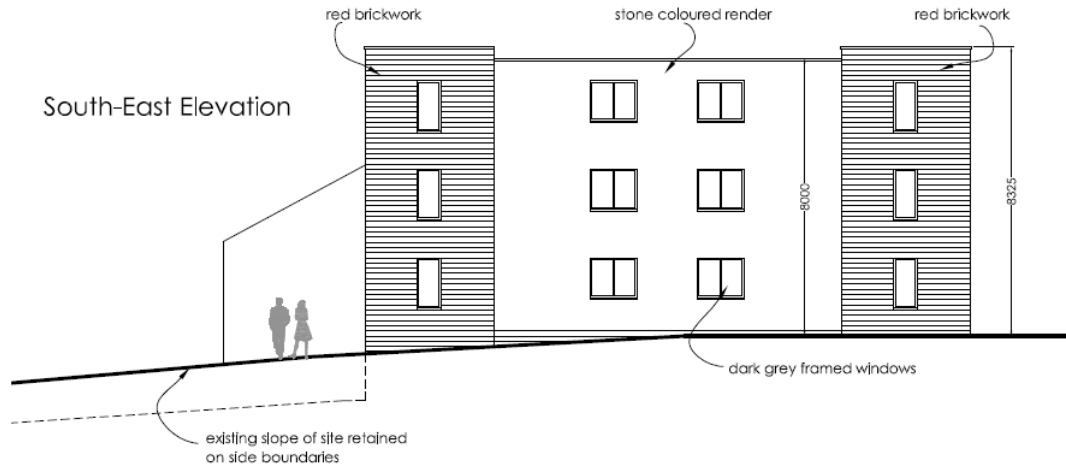


South-West Elevation



North-West Elevation





Proposed Side Elevation

Report of the Chief Executive

APPLICATION NUMBER:	19/00452/FUL
LOCATION:	MILL FARM, 62 MILL ROAD, STAPLEFORD, NOTTINGHAMSHIRE
PROPOSAL:	CHANGE OF USE FROM AGRICULTURAL LAND TO RECREATIONAL USE, CONVERSION OF EXISTING BARN TO COFFEE AND FARM SHOP WITH SITE MANAGERS ACCOMMODATION, DECKING, PARKING AND PROVISION OF AN ADDITIONAL 3 GLAMPING PODS AND RETENTION OF BBQ HUT

Councillor R D MacRae has requested this application be determined by Planning Committee.

1 Executive Summary

- 1.1 The application seeks permission to change the use of the land from agricultural to a recreational use, convert the existing agricultural barn to a coffee/farm shop including external alterations and with first floor manager’s accommodation, the provision of decking, parking, an additional three glamping pods and the retention of a BBQ hut. An application for a change of use from agricultural land to recreational use including six glamping pods was approved by Planning Committee in July 2018 and has already been implemented.
- 1.2 The site is set within the Nottinghamshire Green Belt and Flood Zones 2 and 3. The site consists of a large agricultural building and the land surrounding it is used for general storage and agriculture. Six glamping pods and a BBQ hut with a separate driveway are positioned at the north of the site. The site is surrounded by 2m high metal and timber fencing.
- 1.3 The main issues relate to whether or not the proposal would constitute appropriate development in the Green Belt including the impact on openness, if the development is acceptable in terms of flood risk, highway safety and parking and whether there will be an unacceptable impact on neighbour amenity.
- 1.4 The benefits of the proposal would mean there is a contribution to the local economy of Stapleford through the extension of the existing outdoor recreational use (and proposed associated uses) and re-use/extension/alteration of an existing building which is appropriate development as set out in Section 13 of the National Planning Policy Framework (NPPF) (2019) and Policy 8 of the Part 2 Local Plan (2019). Furthermore, the proposal would be in accordance with policies contained within the development plan which is given significant weight. There would be some impact on neighbour amenity and increase in traffic but these matters are considered to be outweighed by the benefits of the scheme.

APPENDIX

1 Details of the Application

- 1.1 The application seeks permission to convert the existing agricultural barn to a coffee/farm shop with first floor manager's accommodation. Part of the agricultural building will be retained as storage. The coffee shop will have a kitchen and store, serving area, three toilets (including a disabled toilet) and indoor seating area. The farm shop will adjoin the coffee shop internally. The coffee shop will lead out to a decked area which wraps around the north and west of the building which will be elevated 0.3m with a 1.2m high timber railing. The opening hours proposed for the coffee/farm shop are 09:30 – 17:00 Monday to Saturday and 10:00 – 15:00 on Sunday. Manager's accommodation is proposed at first floor level which will have two bedrooms, a bathroom, lounge/kitchen and balcony. The balcony will have an outlook to the west of the site, will be 'cut in' to the roof and will not project beyond the west elevation of the building.
- 1.2 The north, east and west elevations of the barn will be finished with vertical timber cladding and a brick plinth. Part of the west elevation will be finished with a cream render. Four roof lights and three sun tubes will be inserted in the east and west roof slopes which will serve the first floor accommodation. Four doors and three ground floor windows will be inserted in the east elevation servicing the coffee/farm shop and site manager's accommodation and delivery entrances. The north elevation will have bifolding doors and a door which will serve the coffee shop and external toilet entrance. The west elevation will have two sets of bifolding doors serving the coffee shop, a ground floor window and door serving the serving area and a door serving the store area. A BBQ hut has already been constructed on site to the south of the existing glamping pods and is octagonal in shape and 3.6m in height.
- 1.3 One glamping pod is proposed to the north of the site adjoining the existing cluster of pods. Two glamping pods are proposed to the west of the properties on Meadow View. Each pod will be a maximum height of 3.3m and have two bedrooms, kitchen/living/dining area and bathroom. The pod to the north will have two windows in the east elevation, a window and glazed patio doors in the west elevation and a blank north and south elevation. The existing decking adjoining the pod directly to the north will be extended to form a patio for the new pod. The pods to the west of Meadow View will be positioned at an angle meaning they will have two windows in the south east elevation, a window and double doors in the north west elevation and blank north east and south west elevations. A modest sized gravelled area will serve an outdoor seating area to the north west of each of these two pods.
- 1.4 The existing landscaping of the site consists of hardstanding and the proposal is to change this to a gridded surface with gravel with 45 delineated car parking spaces to the north, east and west of the proposed farm/coffee shop. The existing access into the site will be widened to allow two cars to pass side-by-side.
- 1.5 During the course of the application the plans were amended to reduce the number of pods from five to three and elevate them 0.6m above ground level. The two pods next to Meadow View have been increased in size and changed to deluxe pods.

The design of the pods has been amended to include curved timber features and tiled roofs and tile hung sides. The gridded access road serving the two pods next to Meadow View has been relocated to extend beside the east boundary fence with Meadow View. The number of trees has been increased. A small gravelled seating area has been included to the north west of each pod for the two pods next to Meadow View.

2 Site and surroundings

- 2.1 Mill Farm is located on the edge of an urban location, north of Meadow View. The site is within the Nottinghamshire Green Belt and Flood Zones 2 and 3. The land is used for general and farming equipment storage. The farm is mostly surrounded by a 2m high metal and timber fencing. The site consists of a large agricultural barn surrounded by largely hardstanding and some grassed areas. Part of the site to the west and beyond the northern boundary is designated as a Local Wildlife Site.
- 2.2 Residential properties are positioned directly to the south of the farm and beyond a field to the east. Agricultural land mainly surrounds the site and the River Erewash runs adjacent to the west of the site. Six glamping pods are positioned to the north of the site with a separate access driveway which extends beside the east of the site which is accessed from the main site access from Meadow View.
- 2.3 The site is served by two footpaths, one leading north towards Stanton Gate and the other leading west over the River Erewash towards Stanton by Dale.
- 2.4 The site is within walking distance of regular bus services which provide access to Stapleford, Beeston and Long Eaton and is approximately 1.5m miles to junction 25 of the M1.

3 Relevant Planning History

- 3.1 An application to change the use of the land from agricultural to a recreational use, including the siting of 6 glamping pods (18/00328/FUL) was granted permission by Planning Committee in July 2018.
- 3.2 An application to retain a timber framed site office to serve the glamping pod site (19/00042/FUL) was granted permission in March 2019.

4 Relevant Policies and Guidance

4.1 Greater Nottingham Aligned Core Strategies Part 1 Local Plan 2014:

- 4.1.1 The Council adopted the Core Strategy (CS) on 17 September 2014.
 - Policy A: Presumption in Favour of Sustainable Development
 - Policy 1: Climate Change
 - Policy 2: The Spatial Strategy
 - Policy 3: The Green Belt
 - Policy 10: Design and Enhancing Local Identity
 - Policy 13: Culture, Tourism and Sport

4.2 Part 2 Local Plan

4.2.1 The Council adopted the Part 2 Local Plan on 16 October 2019.

- Policy 1: Flood Risk
- Policy 8: Development in the Green Belt
- Policy 13: Proposals for Main Town Centre Uses in Edge-of-Centre and Out-of-Centre Locations
- Policy 17: Place-making, Design and Amenity
- Policy 19: Pollution, Hazardous Substances and Ground Conditions
- Policy 31: Biodiversity Assets

4.3 National Planning Policy Framework (NPPF) 2019:

- Section 2 – Achieving Sustainable Development.
- Section 4 – Decision-making.
- Section 6 – Building a Strong, Competitive Economy.
- Section 9 – Promoting Sustainable Transport.
- Section 12 – Achieving Well-designed Places.
- Section 13 – Protecting the Green Belt.
- Section 14 – Meeting the Challenge of Climate Change, Flooding and Coastal Change.

5 Consultations

5.1 **Council’s Environmental Health Officer:** Initially objected due to concerns in relation to noise and disturbance from the siting of the glamping pods to the east of the site and their proximity to the properties on Meadow View. Advise a note to applicant in respect of requiring a caravan site licence and to contact the Council’s Food and Hygiene team regarding ensuring the internal layout, design and construction of the premises meets particular requirements. Amended plans: no objection but still has concerns over the potential noise and disturbance due to the pods being used for a leisure/holiday use.

5.2 **Council’s Tree Officer:** raise no objection.

5.3 **Council’s Town Centre and Regeneration Officer:** raise no objection. Consider the additional pods and the associated amenities will enhance the existing development by providing further capacity to what appears to be a successful and growing small business which is well outside of Stapleford’s retail centre. The coffee and farm shop elements are not likely to detract from the existing businesses in the town centre and will predominately provide for those staying at the site, not displacing those currently accessing similar services in the town centre. Furthermore, by increasing the amount of possible accommodation at the site, Mill Farm is able to further capitalise on this and provide the business with further credibility as a valuable contribution to the visitor economy, a sector the D2N2 LEP also looks to support growth in. There is no similar tourist offer in the immediate area with such links to green space and local trails, something which should be supported further where possible. As a result of the planned additional visitors to this business, the proposals are considered to be a positive for the local economy.

- 5.4 **Council's Business and Projects Manager Environment:** raise no objection.
- 5.5 **Nottinghamshire County Council as Highways Authority:** no objection in principle subject to conditions relating to pedestrian visibility splays, vehicular footway being widened, surfacing materials and delineation of parking bays.
- 5.6 **Environment Agency:** Object due to insufficient information provided in the Flood Risk Assessment (FRA). Comments in relation to the amended plans are awaited.
- 5.7 17 neighbouring properties were consulted and a site notice and amended site notice were displayed. 31 objections, three letters of support and two observations were received and can be summarised as follows:
- Quiet cul-de-sac of Meadow View will be disrupted and used as an overflow car park
 - Increase in traffic and on-street parking
 - Additional traffic and noise outside of normal office hours and weekends is not acceptable
 - Increase in delivery vehicles and HGV's at uncontrolled times
 - Previous applications for an operating centre for HGV's and trailers was refused on grounds of impact on neighbours
 - Area is already congested with parking
 - Increase in traffic accidents especially involving children and people with mobility issues
 - Double parking on both sides of road which already causes issues with access for emergency vehicles
 - Mill Road is already used as a cut through from Church Street to Derby Road and further traffic would add to this
 - Concerns with access
 - Manor Avenue, Mill Road and other neighbouring roads cannot cope with additional vehicles
 - Mill Road/Meadow View road layout has a bend which means visibility is poor and could cause accidents
 - Access to site should be created from Northwood Street
 - Unsafe for children playing on Meadow View
 - Farm shops, garden centres and leisure/facilities centres are usually positioned on main roads
 - Pods are too close to Meadow View and will cause noise and disruption and loss of privacy, especially given that the pods will have 24/7 access
 - Noise, smell and disturbance
 - Increase in pollution
 - Limited width of access roads and the busy crossroads with B6003 would mean severe disruption during works being undertaken to contaminated soil, also could spread contaminants during removal and transportation
 - Camp site times could be extended beyond the usual 11pm curfew
 - Potential for increase in crime associated with existing isolated houses on Meadow View
 - Contrary to town centre policy on shopping

- Similar developments have increased significantly in size and therefore conditions should be applied to restrict growth
- Concerned by increase in footfall and the impact this would have on local area
- Insufficient consultation with neighbouring properties
- Devalue property
- Positive for Stapleford to have a farm shop and should support local business
- Applicant has made an effort to tidy up the area
- Support the conversion of the agricultural building to a farm/coffee shop
- Regenerate area.

6 Assessment

6.1 The main issues for consideration are whether or not the change of use of the land from agricultural to a recreational use is acceptable, if the proposal is appropriate development in the Green Belt and if it would preserve the openness of the Green Belt. Also relevant to consider are the issues arising from amenity, design, parking and flood risk.

6.2 **Principle and Green Belt**

6.2.1 Section 13 of the NPPF states that the Green Belt serves five purposes which includes to check the unrestricted sprawl of large built up areas and to assist in safeguarding the countryside from encroachment. Inappropriate development is harmful to the Green Belt and should not be approved except in very special circumstances. Substantial weight should be given to any harm to the Green Belt and very special circumstances will not exist unless the potential harm to the Green Belt by reason of its inappropriateness, and any other harm, is clearly outweighed by other considerations. Whilst the construction of new buildings should be regarded as inappropriate, there are certain exceptions including the provision of appropriate facilities for outdoor recreation as long as they preserve the openness of the Green Belt and do not conflict with the purposes of including land within it. Furthermore, the re-use of buildings, provided they are of a permanent and substantial construction, is considered to be an appropriate form of development in the Green Belt.

6.2.2 In addition to the above, Policy 8 of the Part 2 Local Plan refers to proposals for diversification of the rural economy being supported so long as they comply with paragraphs 83 and 84 of the NPPF. These paragraphs state that decisions should enable the expansion of all types of business in rural areas through the conversion of existing buildings and ensure development is sensitive to its surroundings, does not have an unacceptable impact on local roads and exploits any opportunities to make a location more sustainable (for example by improving the scope for access on foot, by cycling or by public transport).

6.2.3 The approval of the application to change the use of the land to a recreational use, including six glamping pods by Planning Committee in July 2018 (18/00328/FUL), occurred immediately after the NPPF was amended to include the change of use of the land as being appropriate development if it is for a recreational use. Notwithstanding the requirement for planning permission, given the previous determination of the six glamping pods being acceptable and this change in the legislation, the proposal of additional glamping pods in the Green Belt, is

considered to be an appropriate recreational use and therefore there is no requirement to demonstrate very special circumstances.

- 6.2.4 The re-use of the barn is considered to be acceptable in line with paragraph 146 of the NPPF which states that the re-use of buildings of a permanent and substantial construction would constitute appropriate development in the Green Belt. This would include the use of the building as living accommodation. Policy 13 of the Part 2 Local Plan states that permission for retail, leisure or food and drink uses in out-of-centre locations will be granted permission so long as they do not result in a unit with 500 square metres floorspace or more, it is in an area of deficiency and meets local needs and does not result in a significant impact on the vitality and viability of any nearby centre. The proposed café and farm shop is less than 300 square metres in floor space. The Town Centre and Regeneration Officer has raised no objection to the application and has stated that the proposed coffee and farm shop elements are not likely to detract from the existing businesses in the town centre and will predominately provide for those staying at the site and concludes the proposal will be positive for the local economy. To conclude, it is considered the re-use of the barn as a farm/coffee shop will contribute to the economy of Stapleford and is not on a large enough scale to detract from the vitality or viability of the town centre and is acceptable.
- 6.2.5 Paragraph 145 of the NPPF states that new buildings within the Green Belt should be regarded as inappropriate and that an exception to this would be the provision of appropriate facilities (in connection with the existing use of land or a change of use) for an outdoor recreation use. Therefore, the proposal to retain the BBQ hut which is positioned on recreational land and is used in conjunction with the glamping pods is considered to be appropriate development in the Green Belt.
- 6.2.6 As part of the proposal, the existing hard surfacing would be replaced with a porous gridded surface with gravel. This would extend around the proposed coffee/farm shop. The raised decking would wrap around the north west corner of the proposed coffee/farm shop and would provide an outdoor seating area for customers. The majority of the site consists of hardstanding, gravel and uneven surfaces. The introduction of a gravelled porous material is considered to be acceptable given the existing hard surfacing on site and the introduction of soft landscaping to break this up. It is considered the impact on openness of this change in material is acceptable as it will not be dissimilar to what is already there and could still retain a rural appearance. The decking will be raised 0.3m in height and as it is attached to an existing building, it is considered this is a modest additional structure with limited impact on openness. The statement of justification provided with the application explains how the hardstanding areas and decking have been reduced in size to reduce their impact on the openness of the Green Belt whilst still retaining a functional purpose.
- 6.2.7 Openness is an essential characteristic of Green Belt and relates to an absence of development. The lawful use of the the majority of the land is agricultural with the north part of the site having changed to recreational where the glamping pods are sited. The site is surrounded by a 2m high wire and wooden fence but otherwise the site is open. The driveway to the northern glamping pods site is bordered by a 1.5m high timber fence and low level vegetation. All three pods would be no higher than 3.3m (including the 0.6m elevation above ground level) and each have a

footprint of 53m². The BBQ hut is 3.6m in height. The pod to the north would be sited with the existing group of pods and the BBQ hut is positioned in proximity to the existing pods. As part of the previous application, the design of the six pods was deemed acceptable (this will be discussed later) and it was recognised that there would be some impact on the openness of the Green Belt through the construction of these buildings. However, whilst it is acknowledged there will be additional buildings, the deluxe cabin is a modest extension of a development that has previously been approved. Furthermore, the BBQ hut is located on land which has permission for recreational use and is considered to be an acceptable design and modest height as to not be detrimental to the openness of the Green Belt.

- 6.2.8 The siting of the two pods to the west of the residential properties on Meadow View is considered to be acceptable in relation to the impact on the openness of the Green Belt. Furthermore, the proposal has been reduced from four to two pods on this part of the site. Considering the buildings are of a modest scale, are an acceptable design and are sited within close proximity to a built up urban area, it is considered this could be seen as a small extension to the residential road. The existing land is hardstanding and although new buildings would be introduced, there would be further planting and grass to soften their appearance. On balance, it is considered the proposal of buildings of this design and scale in this location would not be detrimental to the openness of the Green Belt.
- 6.2.9 To conclude, it is considered the change of use from agricultural to recreational use, the three additional glamping pods, conversion of the barn to a coffee/farm shop with site manager's accommodation, decking, parking and the retention of a BBQ hut would not have a significant impact on the openness of the Green Belt, and it is also considered that the general character of open countryside would remain unaffected.

6.3 Amenity, Design and Layout

- 6.3.1 During the course of the application the plans were amended to reduce the number of pods from five to three and elevate them 0.6m above ground level. The two pods next to Meadow View were increased in size and changed to deluxe pods. The design of the pods was amended to include curved timber features and tiled roofs and tile hung sides. A modest sized gravelled area will serve an outdoor seating area to the north west of each of the two pods. The gridded access road serving the two pods next to Meadow View was relocated to extend beside the east boundary fence with Meadow View. The number of trees has been increased and an additional seven are proposed to be planted along the eastern boundary of the site with Meadow View.
- 6.3.2 The pod to the north of the site would be positioned beyond the proposed coffee/farm shop and existing properties on Meadow View and Mill Road. Due to the positioning of the deluxe pod, the single storey height and the significant separation distance to nearby dwellings and given that the previous six pods were considered to be acceptable, it is considered this pod would not have a detrimental impact on the amenity of the nearby neighbours.
- 6.3.3 The objections received have not made any specific reference to the BBQ hut. Given the use of the hut for BBQ's, it is likely this is used intermittently and in the

warmer months of the year. Given its positioning next to the existing glamping pods on site and the significant separation distance from nearby dwellings, it is considered this does not have a detrimental impact on the amenity of the nearby neighbours.

- 6.3.4 The Council's Environmental Health Officer initially objected to the siting of the glamping pods due to the potential to create noise and disturbance to nearby properties on Meadow View, given the pods are to be used on a recreational basis. The plans have been amended to reduce the amount of pods from five to three, meaning two pods are positioned beyond Meadow View as opposed to four. This is considered to be acceptable given the separation distance of approximately 11m from the nearest property on Meadow View and the positioning of the proposed outdoor seating areas to the north west of the pods which are orientated away from the nearest houses. The Environmental Health Officer has removed their objection but still has concerns over the potential noise and disturbance due to the pods being used for a leisure/holiday use.
- 6.3.5 The south elevation of the agricultural building adjoins the rear gardens of nos. 8 and 9 Meadow View. No changes are proposed to the south elevation as a result of the conversion and this southern part of the barn will be retained as a store area which will provide a buffer between potential noise and disturbance created from the proposed coffee/farm shop to these residents. Between the south elevation of the converted part of the building and the rear gardens of nos. 8 and 9 Meadow View, there is an approximate 9m separation distance and to the rear elevations of these houses there is an approximate separation distance of 17m. It is considered these separation distances will mean there will not be a detrimental impact on the amenity of these occupants as a result of the proposed coffee/farm shop. It is considered the proposed coffee/farm shop is a sufficient distance from other neighbours that there will not be a detrimental impact on their amenity.
- 6.3.6 The opening hours proposed for the coffee/farm shop are 09:30 – 17:00 Monday to Saturday and 10:00 – 15:00 on Sunday. However, it is considered reasonable and necessary for the hours to be conditioned between 09:00 – 17:30 Monday to Saturday and 10:00 – 17:00 on Sunday. It is considered these are acceptable times that would control the level of traffic and noise associated with the site to minimise disturbance to residents.
- 6.3.7 The manager's accommodation will be positioned above the coffee/farm shop at first floor level. Although there is a proposal of a balcony, this will be 'cut in' to the roof of the existing agricultural building so it will face directly west and overlooking to the south will not be possible. The roof lights will be positioned in the east and west roof slopes and will not cause any detrimental overlooking considering their positioning in the building and modest size. The manager's accommodation is positioned within the building and does not directly adjoin any neighbouring property. It is considered the proposed manager's accommodation will not have a detrimental impact on the amenity of surrounding neighbours.
- 6.3.8 Whilst it is acknowledged the proposal of decking will allow customers to congregate outside, the coffee/farm shop will be conditioned so the opening hours mean it can only be open to customers Monday – Friday 09:00 – 17:30 and Sunday 10:00 – 17:00. Due to the proposed use being a coffee/farm shop, it is unlikely that

there would be a significant amount of noise created by customers on the decking associated with these uses. Furthermore, the decking will be no more than 0.3m in height and wrap around the north and west of the building and at its closest point, will be approximately 25m to the nearest garden on Meadow View. It is considered the decking and its use will not have a detrimental impact on the amenity of the surrounding neighbours.

- 6.3.9 Concerns have been raised in relation to noise, smell and disturbance from the proposed development. The coffee shop will need to meet the requirements of the relevant food and safety legislation and provide adequate ventilation and any noise or odour that is considered to be excessive should be reported to Environmental Health.
- 6.3.10 Whilst it is acknowledged the camp site will be open 24/7, the previously approved application did not impose any restrictions in regards to opening times. Due to the nature of the site, it would be unmanageable to enforce a condition on a holiday camp site for this to close at a specific time. Furthermore, camp sites usually have their own restrictions and people are likely to be asleep in the evening.
- 6.3.11 To conclude, whilst it is acknowledged the use of the site will be changed from agricultural land to a recreational use which may increase noise levels associated with the site to a certain extent, it is considered the separation distances and conditioning the opening hours of the coffee/farm shop will ensure that the noise is kept to an acceptable level.

6.4 Parking

- 6.4.1 It is evident within the consultation responses that there is concern that the development will lead to an increased demand for on-street parking which would be detrimental to the area and that highway safety issues could occur due to the site being served from a cul-de-sac.
- 6.4.2 In relation to assessing the highway impacts of a proposal, paragraph 109 of the National Planning Policy Framework states that development should only be refused on transport grounds where the residual cumulative impacts are severe.
- 6.4.3 Whilst it is acknowledged there will be more cars associated with this site due to the change from agricultural to recreational use, it is considered the amount of parking provided is sufficient to support the development. As a result of this application, there would be 11 glamping pods on site. The amount of parking proposed with this application is 39 spaces which includes four spaces for the glamping pods to the west, two disabled spaces and three operational spaces. The existing six glamping pods to the north are serviced by six spaces. Although the deluxe glamping pod and manager's accommodation does not have specifically assigned parking, it is considered the overall amount of 45 spaces is sufficient that there would not be a detrimental impact on highway safety or on-street parking as a result of this development.
- 6.4.4 Concerns in relation to the location of the site access and it not being appropriate have been raised. Whilst it is acknowledged that the site is served by a cul-de-sac, the Highways Authority has not raised any objections or concerns in relation to this being inappropriate in relation to highway safety. It is considered that Mill Road

leading into Meadow View has sufficient visibility to serve this site. There is a slight bend in Meadow View at the site access point which would reduce the speed of vehicles.

6.4.5 The majority of on-street parking in a residential area such as this would be likely to occur in the evening. It is likely the majority of the traffic associated with the coffee/farm shop (due to the opening hours) will be when the roads are quietest in the daytime, with cars returning to the residential area once the coffee/farm shop is closed. Traffic and parking in residential areas at the weekend will be increased but would likely be more sporadic. The coffee/farm shop is likely to be busiest at the weekend but with the opening hours conditioned and users of the site mostly arriving and leaving at staggered times across the day, it is considered there would not be a detrimental impact on highway safety.

6.4.6 Concerns have been raised in relation to emergency vehicles being blocked by an increase in on-street parking and an increase in large delivery vehicles associated with the site. Any illegal parking should be reported to the police and due to the size of the coffee/farm shop, it is considered there would not be a significant increase in large vehicles which would be dissimilar to large farming vehicles that would have used this entrance. A suggestion has been made that the entrance to the site should be from Northwood Street. However, it is considered the use of the existing entrance is acceptable on highway safety terms.

6.4.7 To conclude, it is considered that a total of 45 car parking spaces on site is sufficient to serve the existing pods and proposed development. Whilst it is acknowledged there will be more traffic entering and exiting Mill Road and Meadow View, it is likely that this will be staggered throughout the day. Furthermore, as the coffee/farm shop will likely generate the most traffic and will be conditioned to be open in the day only when the majority of residents will be at work and with more sporadic traffic at the weekends, it is considered this would not be detrimental to highway safety.

6.5.1 Flood Risk

6.5.2 The site lies within Flood Zones 2 and 3 which is land with a high probability (1 in 100 or greater) of river flooding. A Flood Risk Assessment has been submitted with the application. Paragraphs 155 – 158 of the NPPF states that inappropriate development in areas of high risk of flooding should be avoided but where it is necessary, should be undertaken without increasing flood risk elsewhere. All plans should apply a sequential, risk-based approach to the location of development in order to steer new development to areas with the lowest risk of flooding.

6.5.3 As the site is located within Flood Zones 2 and 3 and is classed as a more vulnerable development type, a sequential test is necessary. The proposed pods are deliberately located within an attractive setting within close proximity to the Erewash Valley Trail and will be an extension of an existing use. It is considered that there are no other reasonable available alternative sites that could benefit from the Erewash Valley Trail that are outside Flood Zones 2 and 3, therefore sequentially, there are no other favourable sites and the development is considered acceptable in this location.

6.5.4 The pods have been elevated 0.6m above ground level in line with the elevated pods in the previously approved application (18/00328/FUL). Condition 6 of the previous application included that the finished floor levels of the pods should be set no lower than 38.78m AOD which will be repeated in condition 4 of the recommendation.

6.5.5 The Environment Agency has objected to the application due to insufficient information. A revised Flood Risk Assessment has been submitted and comments are awaited.

6.6 Other Issues

6.6.1 Concerns were raised in the representations received that an insufficient consultation with neighbours has been carried out. One site notice and an amended site notice was displayed within close proximity to the site and adjoining neighbours to the site were informed of the application by letter. This is in line with statutory requirements and is considered sufficient.

6.6.2 The Town Centre and Regeneration Officer is in support of the scheme and has stated that it is not likely to detract from the existing business in Stapleford town centre and that this development could provide a credible contribution to the visitor economy. It is considered the farm/coffee shop will not have an adverse effect on the vitality and viability of Stapleford town centre due to the floor space being less than 300 sqm and the separation distance from the town centre.

6.6.3 Devaluation of property is not a material planning consideration.

6.6.4 It would be unnecessary to condition the use to restrict further expansion. Any future applications would be dealt with on their own merits.

6.6.5 It is considered the development will not cause an increase in crime as the conversion of the barn to create living accommodation will likely increase surveillance and deter crime.

6.6.6 There is no evidence of previous applications associated with this site being refused in relation to an operating centre for HGV's.

7 Planning Balance

7.1 The benefits of the proposal are that it would make an efficient use of a relatively disused agricultural site that would be in accordance with policies contained within the development plan. Whilst it is acknowledged there will be some impact on the amenity of neighbours and traffic in the local area, it is considered this is outweighed by the benefits of the scheme due to the contribution to the local economy and amendments that have been secured to counteract any severe impact on the surrounding residential area.

8 Conclusion

8.1 To conclude, it is considered that the proposal would be appropriate development in the Green Belt, would not be harmful to the character of this Green Belt location

and whilst it is accepted there would be some impact on openness, it is considered this has been counteracted by the amendments to the design, the reduction in the number of pods and the increase in soft landscaping. It is considered the proposal would have some impact on parking in the area but as the site would be served by a total of 45 parking spaces, it is considered this is sufficient that there would not be a detrimental impact on highway safety. It is considered there will not be a detrimental impact on the amenity of the neighbours. It is therefore considered that the scheme is acceptable and planning permission should be granted.

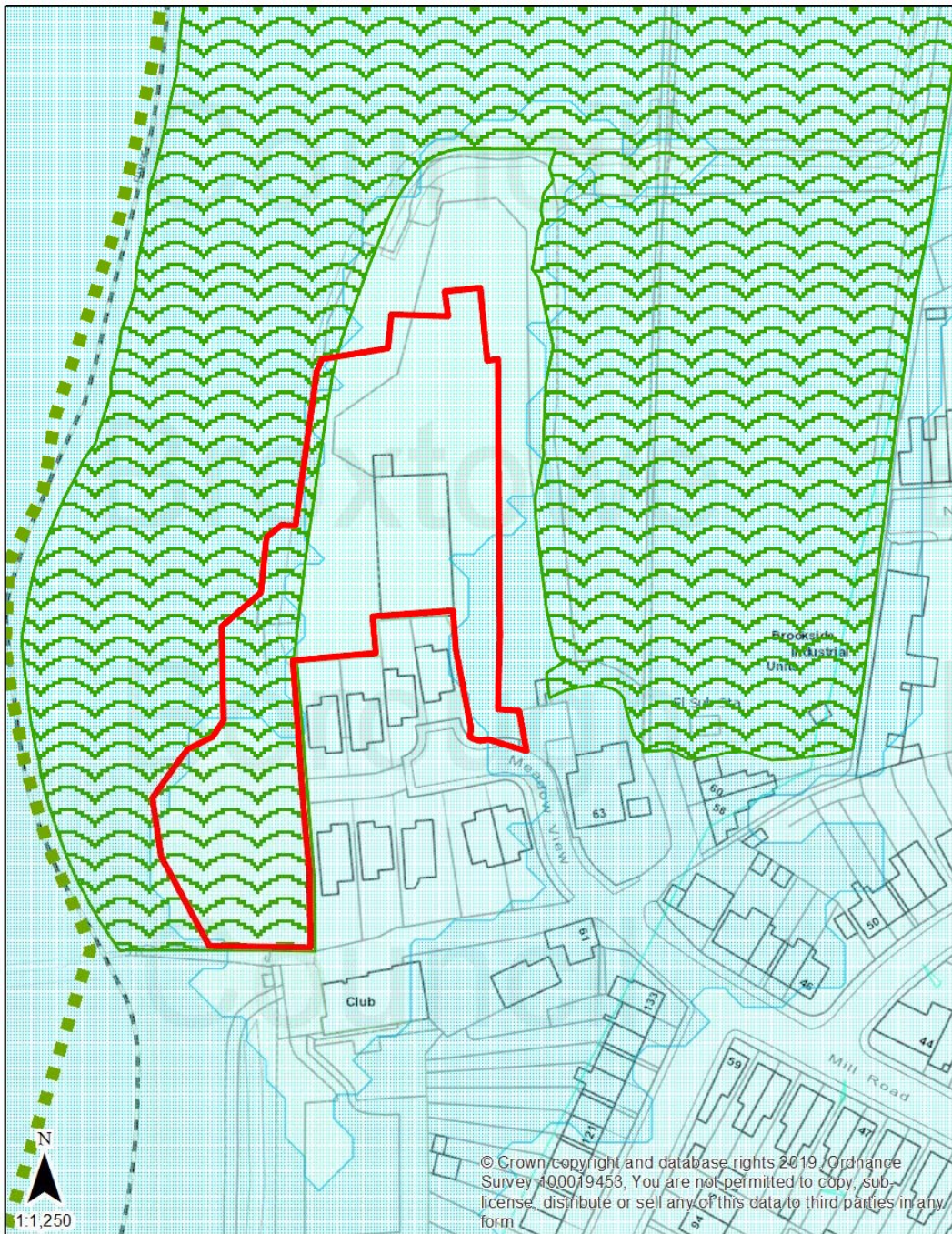
<u>Recommendation</u>	
The Committee is asked to RESOLVE that planning permission be granted subject to the following conditions.	
1.	<p>The development hereby permitted shall be commenced before the expiration of three years beginning with the date of this permission.</p> <p><i>Reason: To comply with S91 of the Town and Country Planning Act 1990 as amended by S51 of the Planning and Compulsory Purchase Act 2004.</i></p>
2.	<p>The development hereby permitted shall be carried out in accordance with drawing numbers: 3459/7 received by the Local Planning Authority on 10 September 2019, 3459/06B received by the Local Planning Authority on 5 November 2019, 3459/03D received by the Local Planning Authority on 13 November 2019, 3459/04D received by the Local Planning Authority on 15 November 2019 and 3459/05H received by the Local Planning Authority on 5 December 2019.</p> <p><i>Reason: For the avoidance of doubt.</i></p>
3.	<p>No building works, including site clearance, shall take place until a landscaping scheme has been submitted to and approved in writing by the Local Planning Authority. The landscaping scheme shall include:</p> <ul style="list-style-type: none"> (a) numbers, types, sizes and positions of proposed trees and shrubs; (b) proposed hard surfacing treatment; (c) planting, seeding/ turfing of other soft landscape areas; (d) details of the site boundary treatments and any pod curtilage boundary treatments; (e) details of any external lighting; and (f) a timetable for implementation of the scheme. <p>The landscaping scheme shall be carried out in accordance with the approved timetable. If any trees or plants, which, within a period of 10 years, die, are removed or have become seriously damaged or diseased they shall be replaced in the next planting season with</p>

	<p>ones of similar size and species to the satisfaction of the Local Planning Authority.</p> <p><i>Reason: No such details were submitted with the application and the development cannot proceed satisfactorily without the outstanding matters being agreed in advance of development commencing and to ensure the development presents a more pleasant appearance in the locality, does not adversely impact on the Nottinghamshire Green Belt, and in accordance with the aims of Policy 10 Broxtowe Aligned Core Strategy (2014) and Policy 8 of the Part 2 Local Plan (2019).</i></p>
<p>4.</p>	<p>Notwithstanding the details on the submitted plans, the glamping pods hereby approved shall have a finished floor level set no lower than 38.78m AOD and the converted barn finished floor levels shall be set no lower than 37.18m AOD. This is in accordance with sections 5.5.1 and 5.5.2 of the accompanying flood risk assessment. These floor levels shall be maintained and retained for the lifetime of the development.</p> <p><i>Reason: To reduce the risk of flooding and in accordance with the aims of Policy 1 of the Broxtowe Aligned Core Strategy (2014) and Policy 1 of the Part 2 Local Plan (2019).</i></p>
<p>5.</p>	<p>The glamping pods and coffee/farm shop hereby approved shall not be brought into use until the dropped vehicular footway has been widened and is available for use, pedestrian visibility splays of 2m by 2m are provided each side of the vehicle access and the parking bays clearly delineated in accordance with drawing reference 3459/04D received by the Local Planning Authority on 15 November 2019. This shall be maintained for the life of the development.</p> <p><i>Reason: To ensure that adequate off-street parking provision is made to reduce the possibilities of the proposed development leading to on-street parking in the area and in accordance with Policy 17 of the Part 2 Local Plan (2019).</i></p>
<p>6.</p>	<p>No pod erected pursuant to this permission shall be occupied or brought into use until:</p> <p>i) All necessary remedial measures identified within the Phase 1 Desktop Study and Contamination Assessment Report (ref IV.84.18) dated April 2018 have been completed in accordance with details approved in writing by the Local Planning Authority; and</p> <p>(ii) It has been certified to the Local Planning Authority that necessary remedial measures have been implemented in full and that they have rendered the site free from risk to human health from the contaminants identified.</p>

	Reason: In the interest of public health and safety.
7.	<p>The glamping pods hereby approved shall only be used for the purposes of holiday letting accommodation and shall not be occupied as permanent residential units at any time.</p> <p><i>Reason: This Green Belt site is not suitable for permanent residential dwellings and for the avoidance of doubt.</i></p>
8.	<p>The coffee/farm shop hereby approved shall not be open to customers except between the hours of 09:00 – 17:30 Monday to Friday and 10:00 – 17:00 on Sunday.</p> <p><i>Reason: In the interests of amenity for nearby residents and in accordance with the aims of the Policy 10 of the Aligned Core Strategy and Policy 17 of the Part 2 Local Plan (2019).</i></p>
	NOTES TO APPLICANT
1.	The Council has acted positively and proactively by working to determine this application within the agreed determination date.
2.	<p>The proposed development lies within a coal mining area which may contain unrecorded coal mining related hazards. If any coal mining feature is encountered during development, this should be reported immediately to the Coal Authority on 0345 762 6848.</p> <p>Further information is also available on the Coal Authority website at: www.gov.uk/government/organisations/the-coal-authority</p>
3.	<p>The development makes it necessary to extend the vehicular crossing over a verge of the public highway. These works shall be constructed to the satisfaction of the Highway Authority. Works will be subject to a design check and site inspection for which a fee will apply. The application process can be found at: http://www.nottinghamshire.gov.uk/transport/licences-permits/temporary-activities</p>
4.	A caravan site license would be required in order to meet the minimum standards that must be achieved to protect health, safety and welfare of site occupants. The applicant should contact the Council's Licensing department on 0115 917 7777.
5.	Before any works are undertaken, the applicant should contact the Council's Food Hygiene team on 0115 917 3485. The premises must be registered and approved by the Council 28 days prior to trading.
6.	Any external extraction systems e.g. flues or air conditioning units are likely to require planning permission.

7.	The prospective site manager should register to receive flood warnings.
8.	A Flood Warning and Evacuation Plan for the site should be brought into place prior to the occupation of the development with this information being made available to any subsequent users.

Site Plan



Legend

-  Site
-  Green Belt
-  Green Infrastructure Corridor
-  Local Wildlife Site
-  Flood Zone 3
-  Flood Zone 2

Photographs



East elevation of agricultural barn



West elevation of no. 6 Meadow View



Rear elevation of nos. 6 – 9 Meadow View



View of site facing south towards entrance, no. 9 Meadow View and agricultural barn

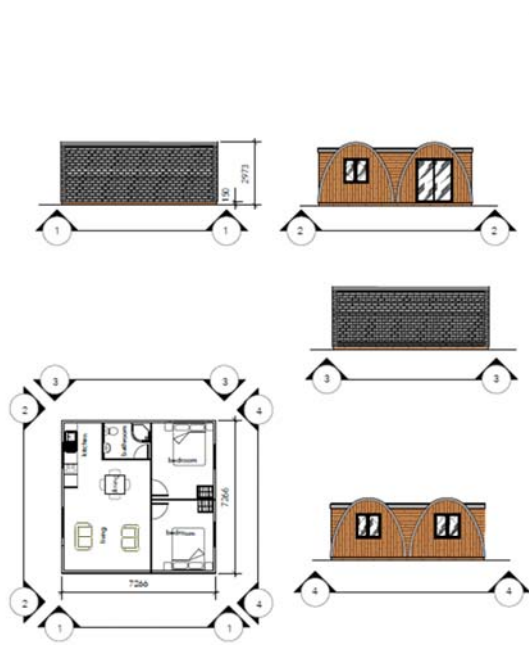
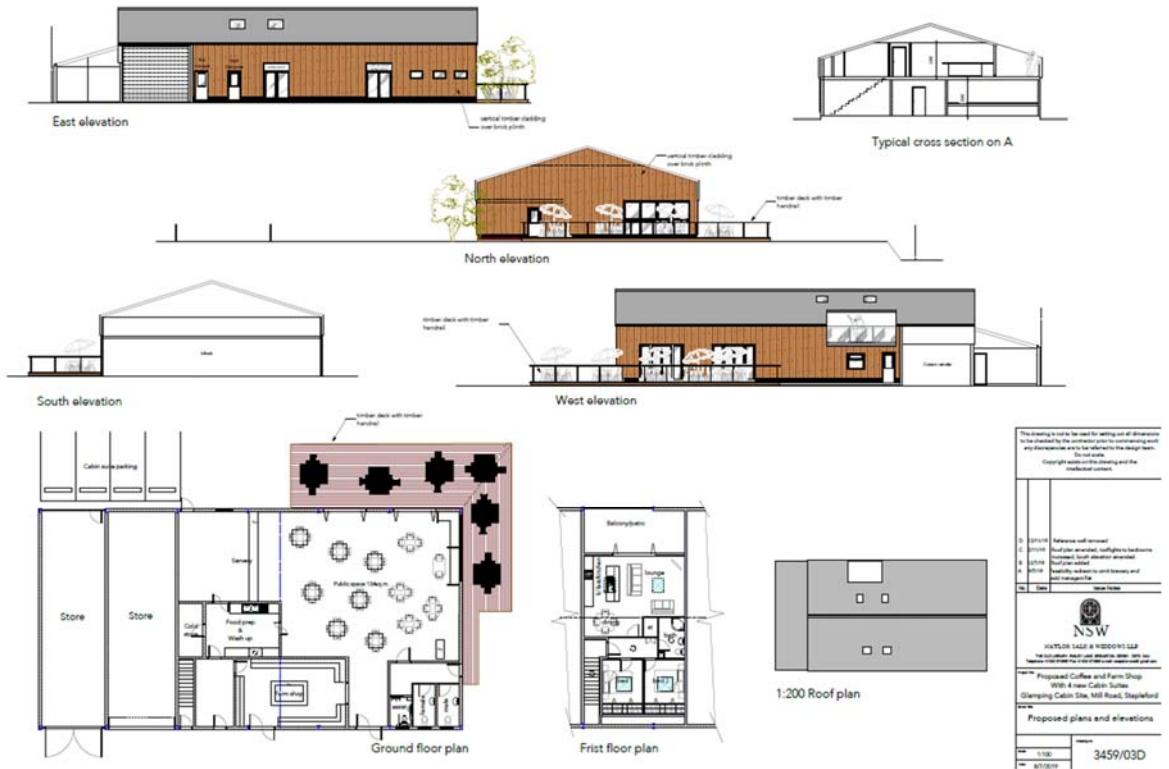


View of Meadow View facing north west



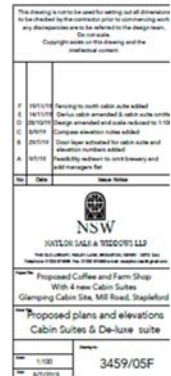
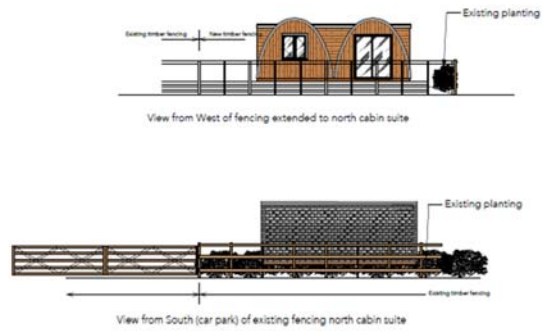
View facing north (BBQ hut and existing pods)

Plans (not to scale)



MATERIALS:
 Walls and roof - Deore dark grey tiles and Cedar t & g boarding.
 Windows and doors - Grey thermally broken double glazed.
 Arch trim - Dark grey prefinished aluminium.

De-luxe Cabin



Report of the Chief Executive

APPLICATION NUMBER:	19/00442/FUL
LOCATION:	45 Town Street Bramcote Nottinghamshire NG9 3HH
PROPOSAL:	Construct two storey side extension and subdivide existing building to form four apartments, construct bin store and erect boundary enclosure and gates.

The application is brought to the Committee at the request of Councillor D K Watts.

1 Executive Summary

- 1.1 The application seeks planning permission for the erection of a two storey side extension to enable the conversion of the existing dwelling and extension to four apartments, comprising two x two bed and two x one bed apartments. The application also includes the construction of a bin store and erection of boundary enclosure and gates. Parking for five vehicles would be provided to the front.
- 1.2 The site is within the Bramcote Conservation Area and adjacent to The White Lion public house, which is a Grade II Listed Building.
- 1.3 The site is currently occupied by a two storey detached property, last in use as a single dwelling, which is accessed via the public house car park.
- 1.4 The main issues relate to whether the principle of the development is acceptable, if the design, scale and massing, and impact on neighbours and highway safety is acceptable, and impact on the Bramcote Conservation Area and the setting of the Grade II Listed Building.
- 1.5 The benefits of the proposal are that it would see the re-use of the existing building, which if left un-occupied could lead to its' further deterioration, providing housing in an existing urban area and would be in accordance with the policies contained within the development plan. This is given significant weight.
- 1.6 The negatives of the proposal are the potential for an impact on the Bramcote Conservation Area and the setting of the listed building, and potential impact on the amenities of the occupiers of nearby property. On balance, it is concluded that the benefits of re-use and development of the building outweigh the less than substantial harm to the heritage assets, and the impact on the amenities of the neighbouring occupiers.
- 1.7 The Committee is asked to resolve that planning permission be granted subject to the conditions outlined in the appendix.

APPENDIX**1 Details of the Application**

- 1.1 The proposal seeks to build a two storey extension to the north west of the existing building. The overall development would then comprise four apartments, two x two beds which would be in the existing building, and two x one beds which would be in the extension. The existing building is Georgian in appearance with a tiled roof which is hipped to the left side, and roughcast render to all elevations. The proposed extension would be of a modern design, with a mono-pitch roof and flat roof linking section. It would be built of buff brick with feature standing seam zinc cladding to the link and the areas around the windows.
- 1.2 The existing building and the extension would each have an entrance serving the ground and first floors. Vehicle access to the site would be via the existing public house car park, as is the current situation. Five parking spaces and an enclosed bin store, proposed to be of timber, would be provided to the front of the site. The front of the site would be enclosed by a 1.4m brick wall, with wrought iron gates.

2 Site and surroundings

- 2.1 The application site is within the Bramcote Conservation Area and was formerly in the same ownership as the public house and is understood to have been sold off. The White Lion, which fronts Town Street, is a Grade II Listed Building, and has an outdoor area to the rear, separated from the car park and the application site by a 2m high red brick / rendered wall. The car park associated with the public house is to the north west of the public house, and accessed from Town Street. This access also leads to the application site, and the owners of the application site are understood to have a right of access across the car park. The car park wraps round the site to the south west and north west.
- 2.2 Beyond the public house car park, and to the north west, is 47 Church Street, a detached two storey dwelling, the rear elevation of which faces the site. There are mature trees to the common boundary with the car park.
- 2.3 To the rear of the site is 19 St Michael's Square, a two storey detached dwelling which is to the north of 45 Town Street. This property is at the head of a cul-de-sac. The front elevation of 19 St Michael's Square faces south east and looks over the existing rear elevation of 45 Town Street and the area immediately outside. There are no habitable room windows on the side (south west) elevation of 19 overlooking the site. This property falls outside of the Bramcote Conservation Area boundary.
- 2.4 To the south west side of Town Street there are detached two storey dwellings which sit in an elevated position above the road. These properties overlook the car park of the public house and the car park access.

3 Relevant Planning History

3.1 There is no relevant planning history for the application site.

4 Relevant Policies and Guidance

4.1 **Greater Nottingham Aligned Core Strategies Part 1 Local Plan 2014:**

4.1.1 The Council adopted the Core Strategy (CS) on 17 September 2014.

- Policy A: Presumption in Favour of Sustainable Development
- Policy 2: The Spatial Strategy
- Policy 8: Housing Size, Mix and Choice
- Policy 10: Design and Enhancing Local Identity
- Policy 11: Historic Environment

4.2 **Broxtowe Part 2 Local Plan**

4.2.1 The Council adopted the Part 2 Local Plan on 16 October 2019.

- Policy 15: Housing Size, Mix and Choice
- Policy 17: Place-Making, Design and Amenity
- Policy 20: Air Quality
- Policy 23: Proposals affecting Designated and Non-designated Heritage Assets

4.3 **National Planning Policy Framework (NPPF) 2019:**

- Section 2 – Achieving Sustainable Development.
- Section 4 – Decision-making.
- Section 12 – Achieving well-designed places.
- Section 16 – Conserving and Enhancing the Historic Environment.

5 Consultations

5.1 **Council's Conservation Adviser:** initial concerns in regard to the design, material palette and concept, which is far removed from the existing building. The roof angle also looks awkward. The link to the extension should provide a clear division and currently would not provide the lightweight appearance required due to the materials and position of some of the windows. It is a positive that the boundary enclosure shows wall and railings. The rear first floor window does not correspond to any other element of design on the rear. Would like to have sight of the materials and these to be agreed prior to a decision. Comments on amended plans and details of materials – have no issue with the materials samples as submitted. In regard to the rear windows and the eaves details of the link building, the adviser is satisfied with the details as submitted as they address the concerns raised.

- 5.2 **Council’s Waste and Recycling Officer:** each apartment would need to be provided with two bins and a bag for glass. The bins need to be presented at the edge of the adopted highway for collection.
- 5.3 **Council’s Parks and Environment Officer:** no objections, happy to comment on any landscaping details once received.
- 5.4 **Council’s Environmental Health Officer:** no objections
- 5.5 A letter of objection was received from the Bramcote Conservation Society:
- Considers the description to be misleading as it is considered that the proposed element is not an extension but a new building housing two apartments.
 - As the frontage of the site would be laid out for parking and for bin storage, and the rear is of minimum width, it is considered that this would result in a limited amount of external amenity space for the occupiers.
 - The existing property is considered to be neatly designed and well-proportioned, sitting within a suitable garden space. The additional building is considered to be out of scale with the existing house and has no relationship with it and does not reflect any design context within the conservation area. The brick colour is not comparable with any local materials and the box like roof is totally out of place, and fails to fit in with the conservation area or the listed building (public house).
 - Concerned regarding the impact of the new building on the occupiers of the property to the rear (19 St Michael’s Square), as it would sit within 2m of the neighbouring dwelling. Overlooking windows in the rear elevation would be very close to the front elevation of this property. It would dominate this gap and cut out southern light leading to a negative impact on the neighbour.
 - In conclusion, the Society supports the principle of bringing the building back into use but considers the proposal to represent over-development, to be poorly designed, leaving the property surrounded by parking, and to have a negative impact on neighbouring amenity.
- 5.6 Six properties either adjoining or opposite the site were consulted and a site notice was displayed. Five responses were received, two of support, one with observations and two of objection.

Observation:

- The vehicles associated with the apartments would not be able to use the public house car park. This may lead to a problem in the village which is already saturated with parking day and night.

Objections:

- Windows to the rear elevation to the first floor extension would overlook adjacent property, resulting in loss of privacy and increase in noise due to proximity of the extension.
- Design and materials proposed for the new extension are inappropriate, and are out of character compared to the existing building and buildings locally.
- Due to severe problems with traffic along Town Street, any redevelopment should be limited to a single dwelling, as whilst five parking spaces are

proposed, realistically dwellings appear to attract at least two cars each. The streets around the site already have parking problems and the proposal would exacerbate this.

- The vehicle access from the public house car park is considered to be extremely dangerous and pedestrians will be put at an increased danger as this is the only footpath on this side of the road, which attracts pedestrians with children attending nearby schools.

6 Assessment

6.1 The main issues for consideration are the design, scale and massing of the extension; the impact on the character and appearance of the Bramcote Conservation Area and the setting of the Grade II listed building; and impact on the amenities of the occupiers of neighbouring property.

6.2 **Principle**

6.2.1 Aside from its' allocation as being within a conservation area, the site is not allocated for any specific purpose within the Broxtowe Part 2 Local Plan. Apart from the public house, the character of the area is residential. The principle of residential development on this site is considered acceptable, subject to the considerations set out below.

6.3 **Design, Scale and Massing**

6.3.1 The scale and massing of the proposed extension, being two storey and of a similar height to the adjacent property (19 St Michael's Square) and a lower height than the existing property, is considered to be acceptable and it is considered that the extension would appear subservient to the host property.

6.3.2 The design of the extension is of a modern contemporary style, and the materials, which include buff/brown bricks, brown zinc standing seam cladding, and black aluminium window frames, would contrast with the cream render and slate roof of the existing building. Details of the materials have been submitted, accompanied by an explanation of the choice of materials and are considered acceptable as they are a reflection of the palette of materials found in the locality, and the choice of brick colour is referred to as an identified architectural material used in Zone 2 (Old Village core) of the conservation area as set out in the Bramcote Conservation Area Character Appraisal. The modern extension has an inset link element to the existing which provides a visual break, allowing for the existing building to be appreciated whilst not overwhelmed by the new extension.

6.3.3 The boundary enclosure to the front of the site has been amended to show a brick wall with wrought iron gates, which reflects the boundary enclosure style prominent in the local area, and is therefore considered to be acceptable.

6.4 Heritage

- 6.4.1 Paragraph 195 of the National Planning Policy Framework (NPPF) (2019) states: “Where a proposed development will lead to substantial harm to (or total loss of significance of) a designated heritage asset, local planning authorities should refuse consent, unless it can be demonstrated that the substantial harm or total loss is necessary to achieve substantial public benefits that outweigh that harm or loss.”
- 6.4.2 Paragraph 196 of the NPPF states: “Where a development proposal will lead to less than substantial harm to the significance of a designated heritage asset, this harm should be weighed against the public benefits of the proposal including, where appropriate, securing its optimum viable use.”
- 6.4.3 The White Lion PH is a Grade II listed building, and the site and the public house are within the Bramcote Conservation Area. It is considered that the development, being set back from the road, and seen in the context of the public house car park and houses beyond, would not have a significant impact on the character and appearance of the conservation area or the setting of the listed building. Whilst the development would be considered to lead to less than substantial harm, it is considered that the development would bring back into use a vacant building, and this would be considered a public benefit which would outweigh the harm in this case.

6.5 Amenity

- 6.5.1 19 St Michael's Square is the closest residential property to the proposed development. This is a two storey detached property which is side on to the application site. There are no windows in the elevation facing the site. Whilst the proposed extension would be a minimum of 3m from the side elevation of this property, it is noted that the majority of the extension would be set further back from the front elevation of no. 19, and that only the first floor landing window would have the potential to look over the front elevation windows of 19. There would also be an additional window at first floor in the existing building, which serves a bathroom. A condition to ensure that both these windows would be obscurely glazed, and non-opening below 1.7m internal floor level would safeguard the privacy of the occupiers of this property. The proposed extension, due to its position and relationship to the adjoining site, is not considered to have an unacceptable impact on daylight and sunlight reaching this property.
- 6.5.2 It is considered that the occupiers/users of the White Lion PH would not be affected due to the extension being to the opposite side of the existing building to the public house building.
- 6.5.3 It is considered that the occupiers of 47 Church Street would not be significantly affected due to the distances between (a minimum of 22m) and the presence of the public house car park between.
- 6.5.4 The apartments would be of satisfactory internal space standards, and would be compliant with the minimum internal floor areas set out in the DCLG's

Technical Housing Standards guidelines. The proposed occupiers of the apartments would all have access to natural light and to an outlook.

6.5.5 Whilst the amount of external amenity area is constrained, it is considered that the occupiers would have access to public open space in close proximity to the site, at King George’s Park, and a little further afield at Bramcote Park. The occupiers of the two ground floor apartments would each have access to a small private amenity area to the rear.

6.5.6 The site is not in an Air Quality Management Area. In addition, the approval of residential developments that are within walking distance of tram, train and bus provision is considered to have a positive benefit of wider air quality issues by encouraging public transport use and therefore reducing the need to travel by private car.

6.6 Access and Parking

6.6.1 The parking area and layout can accommodate parking for five vehicles, which is considered to be adequate to serve the development of four apartments. There is access to the rear gardens which would allow for the occupiers to have access to store cycles. Whilst comments have been made in regard to on-street parking in the locality being a problem, and that overflow parking would not be given permission within the public house car park from the public house operators, it is considered that the development would not result in a significant increase in demand for on-street parking in the area. Any unauthorised parking would be a matter for the operators of the public house to control.

6.6.2 In regard to the safety of the car park access, it is considered that, as the access would not be altered, and that the increase in traffic movements would be negligible, the proposed use would not have a significant impact on the safe access or exit associated with the development.

6.7 Waste and Recycling

6.7.1 The bin store, proposed to be of timber cladding, is to be located to the front of the application site, adjacent to the parking area. The bin store is of a sufficient size to accommodate eight bins plus space for glass recycling. As such it is considered that the bin store as proposed is acceptable both in terms of its provision and in terms of its position. Details of the materials of the bin enclosure would be secured by condition.

7 Planning Balance

7.1 The benefits of the proposal are that it would see the redevelopment of an unoccupied site; would provide housing within an existing urban area; and would be in accordance with the policies contained within the development plan. This is given significant weight.

7.2 The negative impacts are the impact on the Bramcote Conservation Area and the setting of the Grade II Listed Building.

7.3 On balance, and for the reasons set out above, it is concluded that the benefits of re-use and development of the building outweigh the less than substantial harm to the heritage assets, and the impact on the amenities of the neighbouring occupiers.

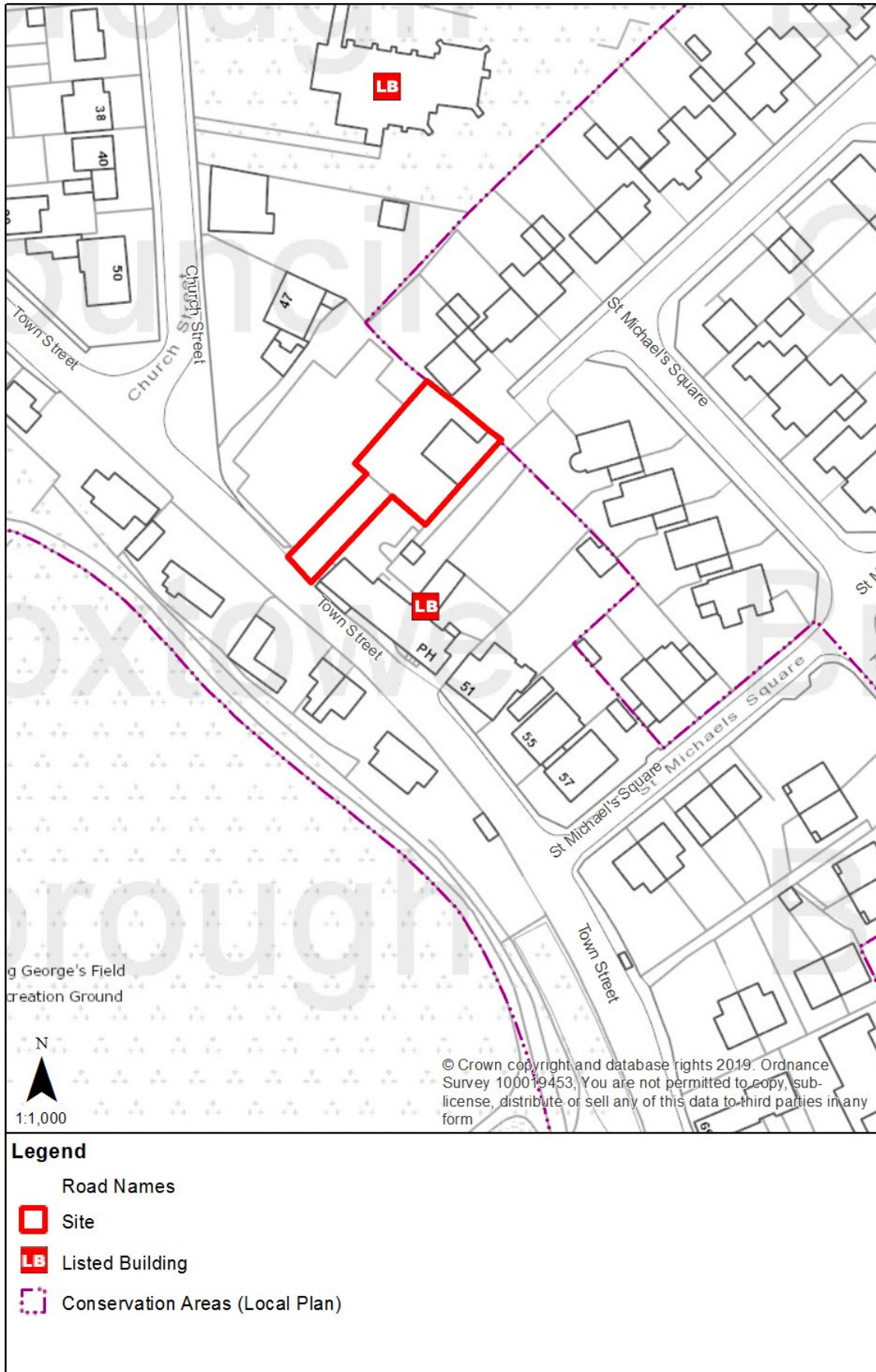
8 Conclusion

8.1 The proposed development to provide four apartments is considered to be acceptable and whilst considered to lead to less than substantial harm to the heritage assets, it is considered that the development would bring back into use a vacant building, and would be considered a public benefit which would outweigh the harm in this case. Subject to the conditions imposed, it is considered that residential amenity will not be unduly affected by the proposals.

<u>Recommendation</u>	
<p>The Committee is asked to RESOLVE that planning permission be granted subject to the following conditions.</p>	
<p>1.</p>	<p>The development hereby permitted shall be commenced before the expiration of three years beginning with the date of this permission.</p> <p><i>Reason: To comply with S91 of the Town and Country Planning Act 1990 as amended by S51 of the Planning and Compulsory Purchase Act 2004.</i></p>
<p>2.</p>	<p>The development hereby permitted shall be carried out in accordance with window details received by the Local Planning Authority on 03.10.19, drawings numbered 004 rev D received by the Local Planning Authority on 21.10.19, 002 rev H and 003 rev G received by the Local Planning Authority on 28.11.19, and the email detailing materials, received by the Local Planning Authority on 21.11.19.</p> <p><i>Reason: For the avoidance of doubt.</i></p>
<p>3.</p>	<p>Notwithstanding the details of materials submitted, no above ground works shall be carried out until details of the manufacturer, type and colour of the bricks and zinc cladding to be used in the facing elevations of the extension, and details of the bin store enclosure materials have been submitted to and approved in writing by the Local Planning Authority, and the development shall be constructed only in accordance with those details.</p>

	<p><i>Reason: Limited details were submitted and to ensure the development presents a satisfactory standard of external appearance, in accordance with the aims of Policies 17 and 23 of the Broxtowe Part 2 Local Plan (2019) and Policy 10 of the Aligned Core Strategy (2014).</i></p>
<p>4.</p>	<p>No above ground works shall take place until a landscaping scheme has been submitted to and approved by the Local Planning Authority. This scheme shall include the following details:</p> <ul style="list-style-type: none"> (a) Numbers, types, sizes and positions of any proposed trees and shrubs (b) Planting, seeding/turfing of other soft landscaping areas (c) Proposed hard surfacing treatment <p>The approved scheme shall be carried out in accordance with the agreed details.</p> <p><i>Reason: Limited details were submitted and to ensure the development presents a satisfactory standard of external appearance, in accordance with the aims of Policies 17 and 23 of the Broxtowe Part 2 Local Plan (2019) and Policy 10 of the Aligned Core Strategy (2014).</i></p>
<p>5.</p>	<p>The approved landscaping shall be carried out not later than the first planting season following the substantial completion of the development or occupation of the building, whichever is the sooner and any trees or plants which, within a period of 5 years, die, are removed or have become seriously damaged or diseased shall be replaced in the next planting season with ones of similar size and species to the satisfaction of the Local Planning Authority, unless written consent has been obtained from the Local Planning Authority for a variation.</p> <p><i>Reason: To ensure the development presents a more pleasant appearance in the locality and in accordance with Policies 17 and 23 of the Broxtowe Part 2 Local Plan (2019) and Policy 10 of the Aligned Core Strategy (2014).</i></p>
<p>6.</p>	<p>The windows on the first floor north east (rear) elevation serving the landing (in the new extension) and the bathroom (to flat 2, in the existing rear elevation) shall be obscurely glazed to Pilkington Level 4 or 5 (or such equivalent glazing which shall first have been agreed in writing by the Local Planning Authority), and be non-opening below 1.7m internal floor level. These windows shall be retained in this form for the lifetime of the development.</p>

	<i>Reason: In the interests of privacy and amenity for nearby residents and in accordance with the aims of Policy 17 of the Broxtowe Part 2 Local Plan (2019) and Policy 10 of the Aligned Core Strategy (2014).</i>
	NOTES TO APPLICANT
1.	The Council has acted positively and proactively in the determination of this application by working to determine it within the agreed determination timescale.
2.	The proposed development lies within a coal mining area which may contain unrecorded coal mining related hazards. If any coal mining feature is encountered during development, this should be reported immediately to the Coal Authority on 0345 762 6848. Further information is also available on the Coal Authority website at: www.gov.uk/government/organisations/the-coal-authority



Photographs



The application site. Access is shown to the front



View of the site across the car park, taken from Town Street



Front elevation of 45 Town Street



Side garden. 19 St Michael's Square can be seen in the background



Side of the site, taken from the west of the car park



Front garden



View of the White Lion PH from the car park



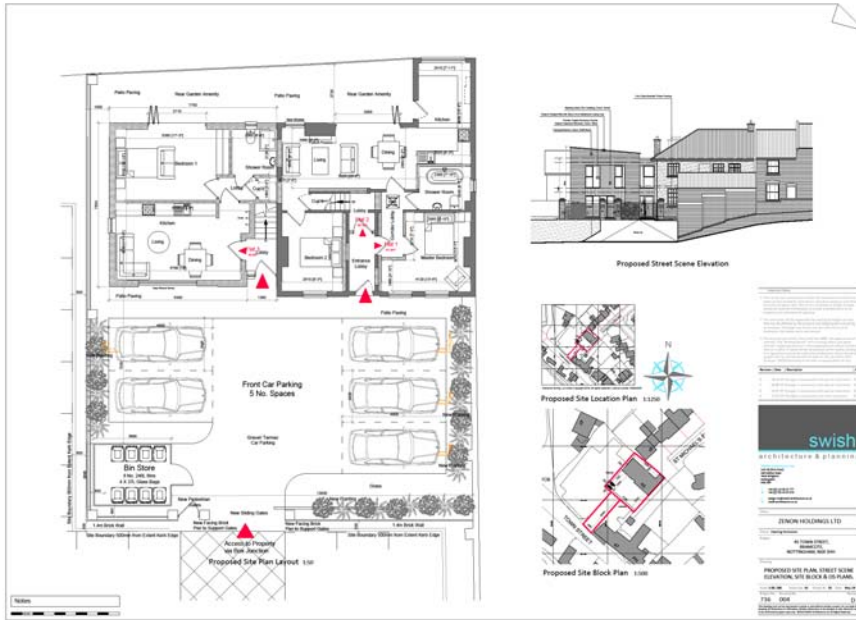
View from the car park toward Town Street



View toward rear of the site from St Michael's Square



Rear elevation



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Report of the Chief Executive

APPEAL DECISION

APPLICATION NUMBER:	18/00516/FUL
LOCATION:	147 - 151 QUEENS ROAD BEESTON NOTTINGHAMSHIRE NG9 2FE
PROPOSAL:	ERECT 10 ONE BEDROOM APARTMENTS

APPEAL DISMISSED

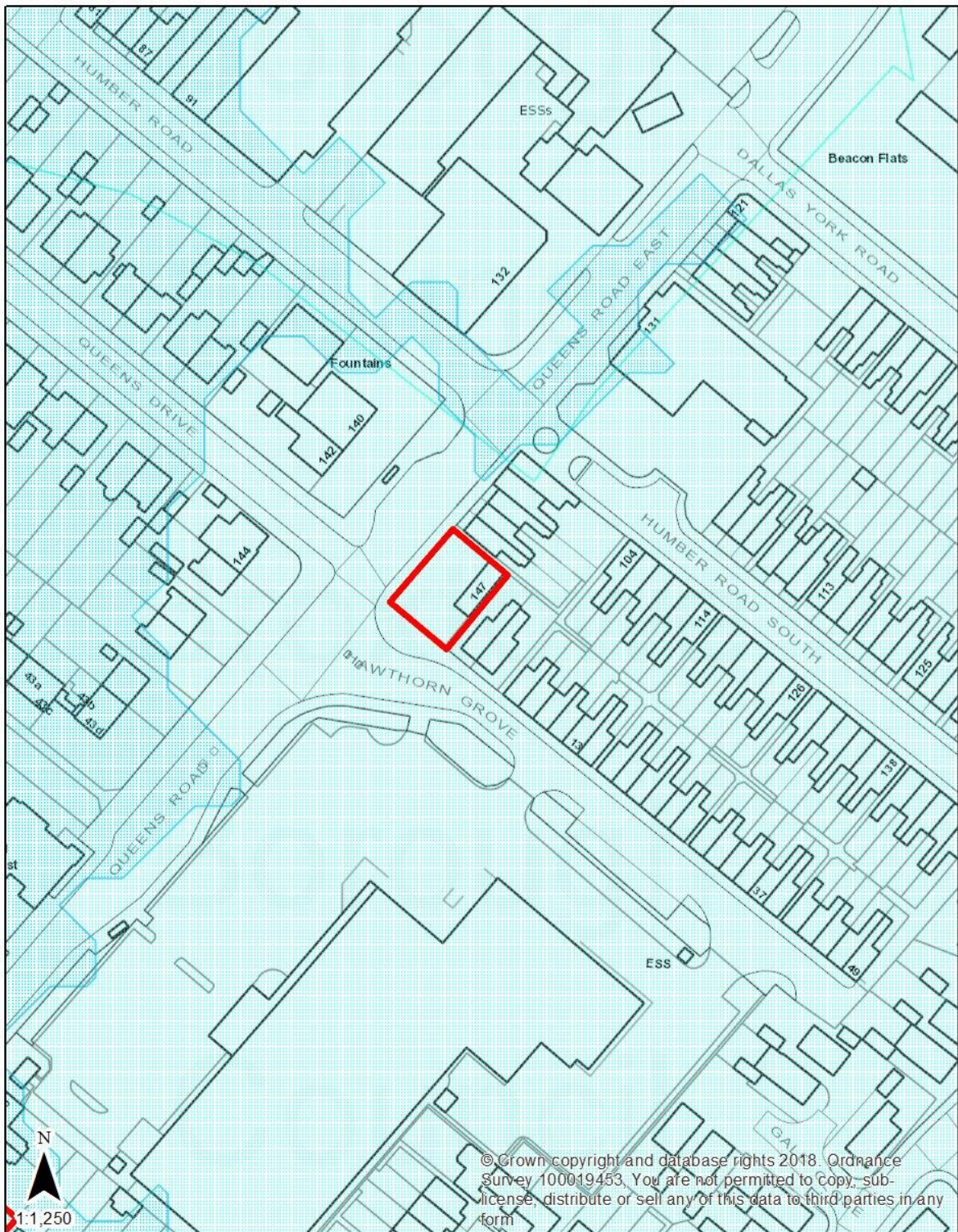
The application to erect 10 one bedroom apartments was refused at Planning Committee on 15 March 2019 for the following reasons:

The proposed building by virtue of its size and scale represents an over intensive development of the site which would be out of keeping with the character of the area. Insufficient parking has been provided on site which would result in parking problems in the immediate area. Accordingly, the proposal is contrary to the aims of Policies H7 and T11 of the Broxtowe Local Plan (2004), Policy 10 of the Broxtowe Aligned Core Strategy (2014), and Policy 17 of the Draft Part 2 Local Plan (2018).

In regards to the reason for refusal concerning the scheme being over-intensive and out of character, the Inspector concluded that the scale and height of the building would be broadly in keeping with the surrounding properties which are a mix of commercial and residential properties and that its position on a main road provides an opportunity for a taller element on the corner. The Inspector concluded that as there is a mix of architectural styles and built forms, with a number of flat roof buildings close by, the building would appear well designed and not unduly dominant in its surroundings and therefore would not harm the character and appearance of the area.

In regards to the reason for refusal concerning parking, the Inspector concluded that the site is in an accessible location which would discourage the use of the private car and as the apartments would be likely occupied by 1-2 people, this would not generate a significant parking requirement. It was concluded that the development would not lead to a rise in nuisance or dangerous parking in the area and a sufficient amount of parking had been provided.

Notwithstanding the point that the Inspector rejected the Council's reasons for refusal he concluded that the scheme should be dismissed based on inadequate living conditions for future occupiers with regard to internal living space. All of the proposed apartments would have a Gross Internal Area (GIA) below what is recommended in the Government's Technical Housing Standards and although these standards are not adopted in Broxtowe's policies, they are useful as a reference point. The Inspector concluded that five out of the ten apartments were unacceptably small and he dismissed the appeal for this sole reason.



Legend

-  Site
-  Flood Zone 3
-  Flood Zone 2

Report of the Chief Executive

UPDATE REPORT ON APPEAL STATISTICS IN RELATION TO THE CRITERIA FOR DESIGNATION OF LOCAL PLANNING AUTHORITIES.

1. Background

This matter was initially reported to members in January 2017 and an update was provided in September 2019. Members of the committee at the September meeting requested a full two-year summary of appeal decisions with any major applications highlighted. This is included in the appendix.

As reported to the 4 September 2019 meeting appeal overturns of five major applications in the preceding two years is almost certain to result in the Council being designated as 'underperforming' and therefore placed into 'special measures'. If this were to occur, it would have serious reputational and financial implications which were summarised previously and are not repeated here. At the time of drafting this report there have been no appeals allowed for major planning applications in the preceding two years. There are no major applications on this committee agenda.

Recommendation

The Committee is asked to NOTE the report.

Background papers

Nil

APPENDIX

Appeal Decisions

Decision Type	Allowed	Dismissed	Split Decision	Total
Refusals by Officers under delegated powers	3	14	1	18
Refusals by Committee contrary to officer recommendation	9	6	0	15
Total Appeal Decisions	12	20	1	33

There have been no appeal decisions on major planning applications since October 2017.

BROXTOWE BOROUGH COUNCIL DEVELOPMENT CONTROL – PLANNING AND ECONOMIC DEVELOPMENT

**PLANNING APPLICATIONS DEALT WITH FROM
26 OCTOBER 2019 TO 22 NOVEMBER 2019**

CONTENTS

Planning applications dealt with under Delegated Powers

Please note: This list is now prepared in WARD order (alphabetically)

BROXTOWE BOROUGH COUNCIL
DEVELOPMENT CONTROL – NEIGHBOURHOODS & PROSPERITY

P L A N N I N G A P P L I C A T I O N S D E T E R M I N E D B Y
D E V E L O P M E N T C O N T R O L

ATTENBOROUGH & CHILWELL EAST WARD

Applicant : C/O AGENT Farland (Nottingham) Ltd 18/00854/MMA
Site Address : Bartons Land Between High Road And Queens Road West High Road Chilwell Nottinghamshire NG9 4AJ
Proposal : **Minor material amendment to planning permission 16/00859/FUL (Hybrid application: Full application to construct 29 dwellings. Outline application (including access) with some matters reserved to construct up to 221 dwellings and units with flexible uses (Classes D1, D2, A1, A2, A3, A4 and A5)) to enable construction of one additional dwelling (full application to construct 30 dwellings)**
Decision : **Conditional Permission**

Applicant : Mr & Mrs Birchall And Mrs Gibson 19/00353/FUL
Site Address : 228 & 230 High Road Chilwell Nottinghamshire NG9 5DB
Proposal : **Construct wall**
Decision : **Conditional Permission**

Applicant : Mr & Mrs Birchall And Mrs Gibson 19/00354/LBC
Site Address : 228 & 230 High Road Chilwell Nottinghamshire NG9 5DB
Proposal : **Listed Building Consent to demolish existing front garden retaining wall and construct replacement wall**
Decision : **Conditional Permission**

Applicant : Mr Steve Brown 19/00521/FUL
Site Address : 35 Farm Road Chilwell Nottinghamshire NG9 5BZ
Proposal : **Construct single/two storey rear extension and insert ground floor window in the south east side elevation**
Decision : **Conditional Permission**

Applicant : Mr James Downs 19/00565/FUL
Site Address : 21 Long Lane Attenborough Nottinghamshire NG9 6BG
Proposal : **Construct boundary wall**
Decision : **Conditional Permission**

AWSWORTH, COSSALL & TROWELL WARD

Applicant : Miss Gemma Baird 19/00612/FUL
Site Address : 19 Barlow's Cottages Lane Awsworth Nottingham NG16 2QW
Proposal : **Retain change of use of outbuilding for beauty business**
Decision : **Conditional Permission**

BEESTON CENTRAL WARD

Applicant : Mr S Baldwin Top 365 Ltd 19/00345/MMA
Site Address : 52 Queens Road East Beeston Nottingham NG9 2GS
Proposal : **Minor Material Amendment to planning permission 16/00849/FUL to retain roof and dormer alterations, revisions to windows and revisions to rear ground levels**
Decision : **Conditional Permission**

Applicant : Mr Dave King 19/00617/OUT
Site Address : 15 Lambeth Court Beeston Nottinghamshire NG9 2DT
Proposal : **Outline application to construct dwelling with all matters reserved**
Decision : **Conditional Permission**

BEESTON NORTH WARD

Applicant : Mr David Miller 19/00578/FUL
Site Address : 55 Clifford Avenue Beeston NG9 2PX
Proposal : **Construct new conservatory roof**
Decision : **Conditional Permission**

Applicant : Mr Jemal Mazlum 19/00636/CLUP
Site Address : 214 Wollaton Road Beeston Nottinghamshire NG9 2PH
Proposal : **Certificate of lawful development for a proposed change of use from dwelling to house in multiple occupation**
Decision : **Approval - CLU**

Applicant : Mr Andrew Owusu & Ms Sara Newbold 19/00653/CLUP
Site Address : 40D Derby Road Beeston Nottinghamshire NG9 2TG
Proposal : **Certificate of proposed development to construct single storey side and rear extensions**
Decision : **Approval - CLU**

BEESTON RYLANDS WARD

Applicant : Mr Phil Ashford Comfort Developments Ltd 19/00201/FUL
Site Address : 68 Trafalgar Road Beeston Nottinghamshire NG9 1LE
Proposal : **Change of use from commercial (Class A1 & A2) with flat above, to two dwellings and construct hip to gable end roof extension, external alterations, boundary wall, fence and gates.**
Decision : **Conditional Permission**

BEESTON WEST WARD

Applicant : Ms S Forster 19/00505/FUL
Site Address : 15 Hope Street Beeston Nottinghamshire NG9 1DJ
Proposal : **Construct single storey rear and first floor side extensions**
Decision : **Conditional Permission**

Applicant : Ellis-Fermor & Negus Solicitors 19/00564/FUL
Site Address : 2 Devonshire Avenue Beeston Nottinghamshire NG9 1BS
Proposal : **Construct single storey front extension**
Decision : **Refusal**

Applicant : Mr Taylor 19/00576/FUL
Site Address : 5 Richmond Drive Chilwell Nottingham NG9 4EB
Proposal : **Construct first floor front extension and convert garage**
Decision : **Conditional Permission**

Applicant : Mr & Mrs Fallows 19/00595/FUL
Site Address : 14 Hampden Grove Beeston Nottinghamshire NG9 1FG
Proposal : **Construct single storey rear and side extension with adjoining steps**
Decision : **Conditional Permission**

Applicant : Mr & Mrs Mark Greenaway 19/00597/CLUP
Site Address : 136 Park Road North Chilwell Nottinghamshire NG9 4DF
Proposal : **Certificate of lawful development to construct rear dormer**
Decision : **Approval - CLU**

Applicant : Mr & Mrs Cunningham 19/00603/FUL
Site Address : 5 Bramcote Road Beeston Nottingham NG9 1AG
Proposal : **Retain single storey rear extension, side boundary fence and steps (revised scheme)**
Decision : **Conditional Permission**

Applicant	:	Mr P Moore	19/00604/FUL
Site Address	:	33 Georgina Road Beeston Nottinghamshire NG9 1GQ	
Proposal	:	Construct two storey side and single storey rear extensions and extend front canopy roof	
Decision	:	Conditional Permission	
Applicant	:	Mrs Amanda Starr	19/00608/FUL
Site Address	:	17 Clinton Street Beeston Nottinghamshire NG9 1AZ	
Proposal	:	Construct two storey side extension	
Decision	:	Conditional Permission	
BRAMCOTE WARD			
Applicant	:	Mr James Bates North Midland Construction	19/00136/ROC
Site Address	:	88 Cow Lane Bramcote Nottinghamshire NG9 3BB	
Proposal	:	Variation of condition 2 (approved drawings and specification) of planning ref: 18/00337/FUL (Construct two dwellings) to enable ground alterations and increased height (maximum of 1.5m) of proposed dwellings, revisions to landscaping and external alterations	
Decision	:	Conditional Permission	
Applicant	:	Mr & Mrs Gorecha	19/00399/FUL
Site Address	:	47 Thoresby Road Bramcote Nottinghamshire NG9 3EP	
Proposal	:	Construct two storey front and side extension, single storey rear extension and external alterations	
Decision	:	Conditional Permission	
Applicant	:	Mr & Mrs H Pous	19/00481/FUL
Site Address	:	12A Buttermere Drive Bramcote Nottinghamshire NG9 3BL	
Proposal	:	Construct single storey side and rear extension	
Decision	:	Conditional Permission	
Applicant	:	Mr Steven Gee	19/00529/FUL
Site Address	:	109 Derby Road Bramcote Nottinghamshire NG9 3GZ	
Proposal	:	Construct new dwelling following demolition of bungalow	
Decision	:	Conditional Permission	
Applicant	:	Mr Richard Adams Northsands Developments Ltd	19/00513/CLUE
Site Address	:	5 Court Yard Bramcote Nottinghamshire NG9 3DL	
Proposal	:	Certificate of lawfulness for existing development (excavation and foundations for plot 3) to confirm the commencement of work in accordance with planning ref: 15/00879/FUL to construct three houses	
Decision	:	Approval - CLU	
Applicant	:	Mr & Mrs Braun	19/00566/FUL
Site Address	:	35 Ilkeston Road Bramcote Nottingham NG9 3JP	
Proposal	:	Construct single storey rear extension and front porch	
Decision	:	Conditional Permission	
Applicant	:	Mrs Clarke	19/00584/FUL
Site Address	:	115 Arundel Drive Bramcote Nottinghamshire NG9 3FQ	
Proposal	:	Construct rear conservatory	
Decision	:	Conditional Permission	
Applicant	:	Mr & Mrs L Wagner	19/00624/FUL
Site Address	:	12 Marshall Drive Bramcote Nottinghamshire NG9 3LD	
Proposal	:	Construct first floor rear extension	
Decision	:	Conditional Permission	
BRINSLEY WARD			
Applicant	:	Mr Andrew Taylor	19/00526/FUL
Site Address	:	Manor Farm Lodge Hall Lane Brinsley NG16 5AG	
Proposal	:	Change of use of land for the siting of a mobile home for residential purposes	
Decision	:	Refusal	

Applicant : Mr Matthew Smith 19/00593/FUL
Site Address : 66 Mansfield Road Brinsley Nottinghamshire NG16 5AE
Proposal : **Construct single storey rear extension**
Decision : **Conditional Permission**

CHILWELL WEST WARD

Applicant : Mr & Mrs Nice 19/00530/FUL
Site Address : 12 Burton Drive Chilwell Nottinghamshire NG9 5NS
Proposal : **Construct single/two storey rear extension and balcony, raise and extend garage roof including dormer with balcony and external alterations including addition of timber/zinc cladding and render**
Decision : **Conditional Permission**

Applicant : Mr & Mrs Daniel Bartalis 19/00570/FUL
Site Address : 37 Pearson Avenue Chilwell Nottingham NG9 4GQ
Proposal : **Construct single storey side and rear extension (revised scheme)**
Decision : **Conditional Permission**

EASTWOOD HILLTOP WARD

Applicant : Mr Andrew Hankin 19/00549/ROC
Site Address : 237A Nottingham Road Eastwood Nottinghamshire NG16 3GS
Proposal : **Removal of condition 6 to allow external areas to be used by customers of planning ref: 18/00325/FUL**
Decision : **Conditional Permission**

Applicant : Mr Andrew Hankin 19/00614/MMA
Site Address : Pick & Davey 237A Nottingham Road Eastwood Nottinghamshire
Proposal : **Minor Material Amendment to planning permission 18/00325/ful (change of use to micro bar) for external alterations including cladding to building, canopy and boundary wall**
Decision : **Conditional Permission**

EASTWOOD ST MARY'S WARD

Applicant : Nicola Challis Argent Foods Ltd 18/00810/OUT
Site Address : Former Belwood Food Site, 42 - 44 Brookhill Leys Road & 8 Newmanleys Road Eastwood Nottinghamshire NG16 3HZ
Proposal : **Outline application with some matters reserved for residential development including change of use of a single retained property, associated infrastructure and other works, including demolition and alteration to access**
Decision : **Conditional Permission**

GREASLEY WARD

Applicant : Mrs Sally Hind 19/00551/FUL
Site Address : Newlands Farm 201A Main Street Newthorpe Nottinghamshire NG16 2DL
Proposal : **Construct barn**
Decision : **Conditional Permission**

KIMBERLEY WARD

Applicant : Mr G Carlin 19/00540/FUL
Site Address : 6 Lawn Mill Road Kimberley Nottinghamshire NG16 2HD
Proposal : **Construct single storey rear extension and front dormer**
Decision : **Conditional Permission**

Applicant : Kimberley Town Council 19/00559/LBC
Site Address : Chapel Knowle Hill Kimberley Nottinghamshire
Proposal : **Listed Building Consent to erect two security cameras and air conditioning unit**
Decision : **Conditional Permission**

Applicant : Kimberley Town Council 19/00562/FUL
Site Address : Chapel Knowle Hill Kimberley Nottinghamshire
Proposal : **Erect two security cameras and air conditioning unit**
Decision : **Conditional Permission**

Applicant	:	Mrs R Wallis	19/00645/PNH
Site Address	:	58 Hardy Street Kimberley Nottinghamshire NG16 2JX	
Proposal	:	Construct single storey rear extension, extending beyond the rear wall of the original dwelling by 3.3 metres, with a maximum height of 3.7 metres and an eaves height of 2.4 metres	
Decision	:	Prior Approval Granted	
Applicant	:	Mr And Mrs Doidge	19/00647/PNH
Site Address	:	19 Poplar Crescent Nuthall Nottinghamshire NG16 1FE	
Proposal	:	Construct single storey rear extension, extending beyond the rear wall of the original dwelling by 3.3 metres, with a maximum height of 3.715 metres, and an eaves height of 2.25 metres	
Decision	:	Withdrawn	
NUTHALL EAST & STRELLEY WARD			
Applicant	:	Mr Lewis Simpson	19/00527/FUL
Site Address	:	26 Highfield Road Nuthall Nottinghamshire NG16 1BS	
Proposal	:	Construct dwelling	
Decision	:	Refusal	
Applicant	:	Mr & Mrs Kaur	19/00555/FUL
Site Address	:	4 Hillingdon Avenue Nuthall Nottinghamshire NG16 1RA	
Proposal	:	Construct two storey side extension	
Decision	:	Conditional Permission	
Applicant	:	Mr Chris Davis	19/00587/FUL
Site Address	:	14 Highfield Road Nuthall Nottinghamshire NG16 1BS	
Proposal	:	Construct two storey side extension	
Decision	:	Conditional Permission	
Applicant	:	Mr Ashley Brandy	19/00591/FUL
Site Address	:	4 Bloomsbury Drive Nuthall Nottinghamshire NG16 1RJ	
Proposal	:	Construct first floor side extension	
Decision	:	Conditional Permission	
STAPLEFORD NORTH WARD			
Applicant	:	Mr M Hibbert	19/00623/PNH
Site Address	:	11 Trowell Road Stapleford Nottinghamshire NG9 8HB	
Proposal	:	Construct single storey rear extension, extending beyond the rear wall of the original dwelling by 5.4 metres, with a maximum height of 3.91 metres and an eaves height of 2.55 metres	
Decision	:	Prior Approval Not Required	
STAPLEFORD SOUTH EAST WARD			
Applicant	:	Mrs Tara Harding	19/00536/FUL
Site Address	:	88 Sisley Avenue Stapleford Nottinghamshire NG9 7HU	
Proposal	:	Construct front dormer	
Decision	:	Conditional Permission	
Applicant	:	Mrs Gosling	19/00574/FUL
Site Address	:	Scout Hall Rear Of Fire Station Pinfold Lane Stapleford NG9 8DL	
Proposal	:	Site a container unit	
Decision	:	Conditional Permission	
TOTON & CHILWELL MEADOWS WARD			
Applicant	:	Mr Mitra	19/00573/FUL
Site Address	:	21 Epsom Road Toton Nottingham NG9 6HQ	
Proposal	:	Construct single storey rear/ side extension and infill to front creating a porch	
Decision	:	Conditional Permission	

Applicant	:	Mrs Geraldine Grey	19/00581/FUL
Site Address	:	18 Sandhurst Drive Chilwell Nottinghamshire NG9 6NH	
Proposal	:	Variation of condition 2 of planning ref: 92/00679/REM & 92/00680/REM (The garages to be kept available for the accommodation of private vehicles . . .) to allow for the conversion of the garage to habitable room.	
Decision	:	Conditional Permission	
Applicant	:	Mr D Brister	19/00585/FUL
Site Address	:	13 Kirkham Drive Toton Nottinghamshire NG9 6HG	
Proposal	:	Construct single storey side extension	
Decision	:	Conditional Permission	
Applicant	:	Mr & Mrs C Tigg	19/00590/FUL
Site Address	:	11 Stapleford Lane Toton Nottinghamshire NG9 6FZ	
Proposal	:	Construct rear extension, front porch, front dormers, raise the roof to form rooms in roofspace, and extension and alterations to garage	
Decision	:	Conditional Permission	
Applicant	:	Mr & Mrs Jeremy and Emily Farthing	19/00613/FUL
Site Address	:	39 Newall Drive Chilwell Nottinghamshire NG9 6NU	
Proposal	:	Convert garage into habitable room	
Decision	:	Conditional Permission	
Applicant	:	Mr & Mrs Mears	19/00628/FUL
Site Address	:	8 Leigh Road Toton Nottinghamshire NG9 6GF	
Proposal	:	Construct single storey side and rear extensions	
Decision	:	Conditional Permission	
WATNALL & NUTHALL WEST WARD			
Applicant	:	Mr Robin Akers	19/00594/FUL
Site Address	:	24 Watnall Road Nuthall Nottinghamshire NG16 1DU	
Proposal	:	Construct single storey rear extension	
Decision	:	Conditional Permission	
Applicant	:	Mr D Baxter	19/00649/PNH
Site Address	:	38 Philip Avenue Nuthall Nottinghamshire NG16 1EA	
Proposal	:	Construct single storey rear extension, extending beyond the rear wall of the original dwelling by 3.5 metres, with a maximum height of 3.65 metres, and an eaves height of 2.25 metres	
Decision	:	Prior Approval Not Required	

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